

Longview, Texas East Texas Regional Airport Southwest GA Area Taxilane - Phase I

FAA AIP No. 3-48-0137-45-2018

COUNTY BID No. 2018-815

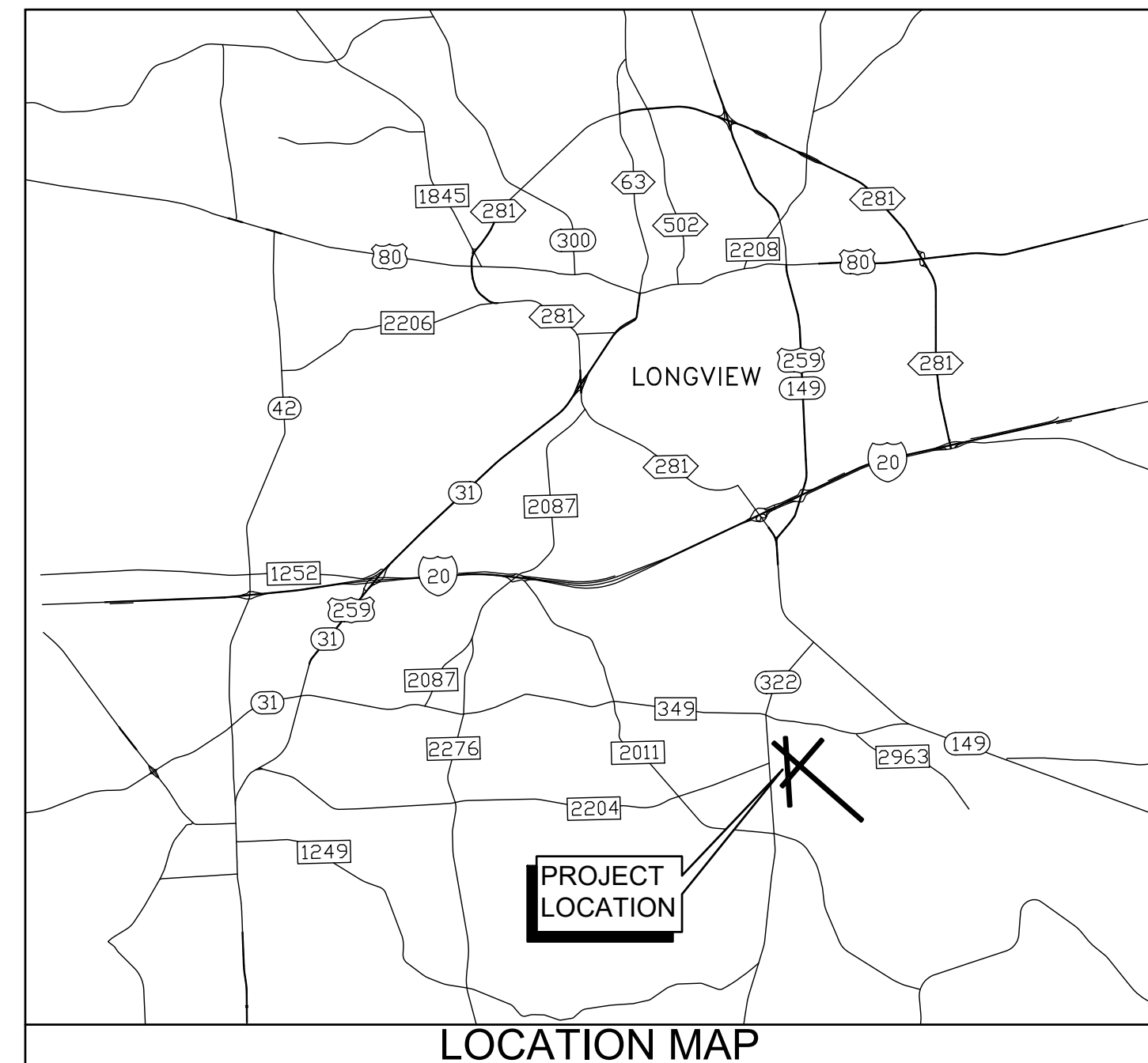
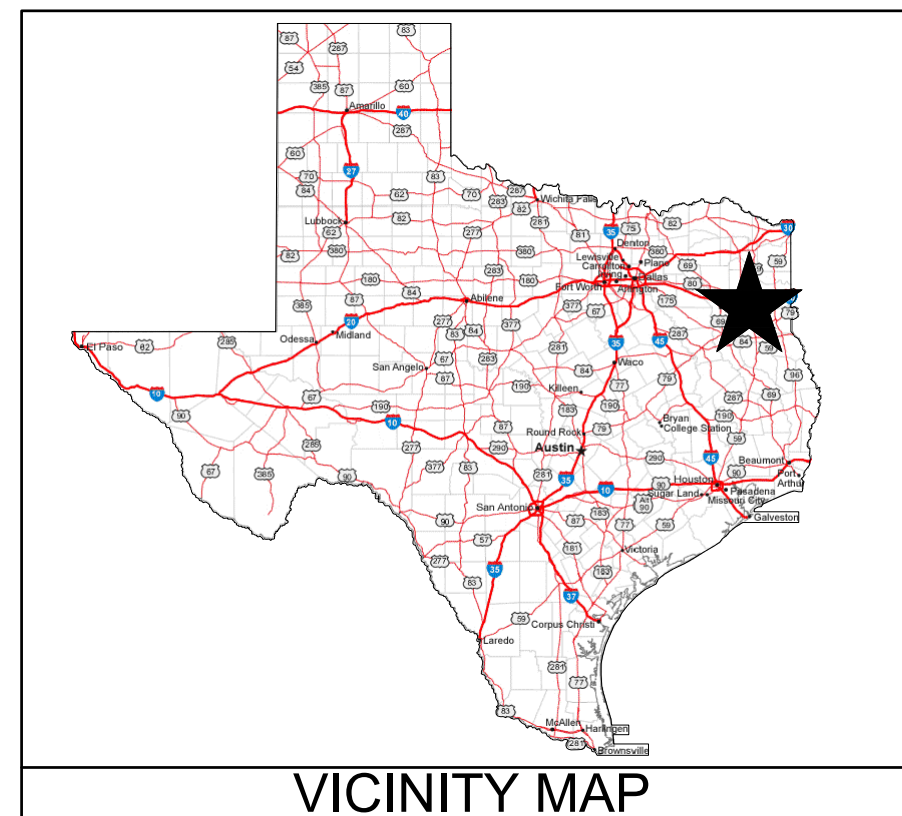
COUNTY OFFICIALS

COUNTY JUDGE
COMMISSIONER - PRECINCT 1
COMMISSIONER - PRECINCT 2
COMMISSIONER - PRECINCT 3
COMMISSIONER - PRECINCT 4

BILL STOUTD
RONNIE L. MCKINNEY
DARRYL PRIMO
GARY BOYD
DARYL WILLIAMS

AIRPORT DIRECTOR

ROY H. MILLER, Jr. A.A.E.

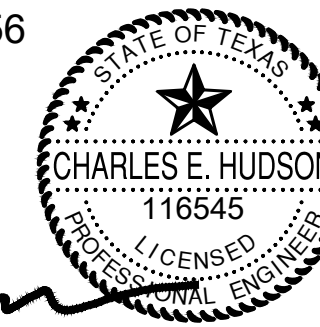


RECOMMENDED BY:

KSA

TBPE FIRM REGISTRATION No. F-1356

Eric Hudson



CHARLES E. HUDSON, P.E.
PROJECT ENGINEER

6/15/18

DATE

ACCEPTED BY:

EAST TEXAS REGIONAL AIRPORT

ROY H. MILLER, Jr. A.A.E.
AIRPORT DIRECTOR

DATE

KSA

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SUMMARY OF QUANTITIES

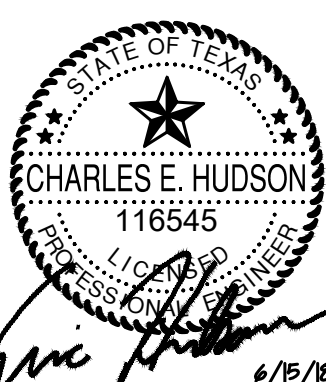
Item No.	Spec. No.	Description	Units	Estimated Quantities
Base Bid				
1.01	P-101-5.1	Pavement and Base Removal	S.Y.	1,625
1.02	P-151-4.2	Clearing and Grubbing	AC.	3
1.03	P-152-4.1	Unclassified Excavation	C.Y.	7,000
1.04	P-155-8.1	Lime-Treated Subgrade	S.Y.	5,800
1.05	P-155-8.2	Lime	LB	315,000
1.06	P-156-5.1	Installation and Removal of Silt Fence	L.F.	1,075
1.07	P-401-8.1	Bituminous Surface Course	TON	770
1.08	P-403-8.1	HMA Base Course	TON	1,150
1.09	P-620-5.1	Runway and Taxiway Markings	S.F.	1,300
1.10	F-162-5.1	6' Chain-Link Security Fence	L.F.	615
1.11	D-701-5.1	18" ASTM C-76, Class III,RCP	L.F.	250
1.12	D-701-5.2	18" ASTM C-76, Class III,RCP, ARCH	L.F.	50
1.13	D-751-5.3	Inlets	EA.	2
1.14	D-754-5.1	Concrete Ditches	C.Y.	50
1.15	T-901-5.1	Seeding	AC.	2
1.16	T-905-5.2	Topsoiling (Furnished From Off the Site)	S.Y.	10,100
1.17	GP-105	Mobilization	L.S.	1
1.18	S-005.1	Geotextile Fabric	S.Y.	2,550
1.19	S-006.1	Erosion Control Matting	S.Y.	1,550
1.20	S-13-5.1	Taxiway Centerline Retroreflective Markers (Green)	EA.	23
1.21	S-33-5.1	Furnish and Install 3" AWWA C900 DR 18 PVC Waterline	L.F.	600
1.22	S-33-5.2	Furnish and Install 3" Tapping Sleeve and Valve Assembly	EA.	1
1.23	S-33-5.3	Install 1" Water Service Line	EA.	1
1.24	S-33-5.4	Furnish and Install 3" Gate Valve Assembly	EA.	1
1.25	TXDOT 340	Fine Graded Hot-Mix Asphalt (Type D)	TON	525
1.26	TXDOT 247-5.1	6" Flexible Base Course	S.Y.	2,800

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SHEET INDEX AND SUMMARY OF QUANTITIES

**EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
LONGVIEW, TEXAS**

DRAWN BY: JKP
DESIGNED BY: CEH
LATEST REVISION: 6/15/2018
KSA JOB NO.: GC.107



SEAL: TBPE Firm Registration No. F-1356
SHEET NO.

SAFETY & SECURITY NOTES

- NO VEHICLE OR EQUIPMENT SHALL MOVE UPON APRONS, TAXIWAYS AND/OR RUNWAYS UNLESS THE VEHICLE:
 - IS IN RADIO CONTACT WITH THE ATCT AT UNICOM FREQUENCY 121.6, OR
 - IS UNDER ESCORT BY A VEHICLE IN RADIO CONTACT WITH THE ATCT, AND
 - IS IDENTIFIED BY MARKING OR A 3"x3" ORANGE AND WHITE CHECKERED FLAG AND FLASHING AMBER LIGHT. MARKINGS ON VEHICLES MUST BE MARKED WITH COMPANY NAME/LOGO THAT IS EASILY READ AT A DISTANCE BY THE ATCT.
- AIRPORT SECURITY IS OF UTMOST IMPORTANCE. THE CONTRACTOR SHALL KEEP THE GATES LOCKED OR GUARDED AT ALL TIMES. THE AIRPORT SHALL PROVIDE THE CONTRACTOR WITH ACCESS TO THE AIRFIELD THROUGH THE GATES.
- IF THE CONTRACTOR WISHES TO CHANGE OR MODIFY THE PARKING AND STAGING AREA THE CONTRACTOR SHALL SUBMIT FOR APPROVAL A PLAN SHOWING PROPOSED PARKING AND STORAGE LOCATIONS. THIS PLAN WILL HAVE TO BE APPROVED BY THE OWNER AND ENGINEER PRIOR TO COMMENCING WORK. THE CONTRACTOR SHALL PARK ALL EQUIPMENT IN THE STORAGE AREA WHEN NOT IN USE.
- FLAGMEN WILL BE REQUIRED TO DIRECT THE CONTRACTOR'S TRUCKS AND EQUIPMENT WHICH ARE OPERATING IN THE AREA OF MOVING AIRCRAFT.
- CONSTRUCTION PERSONNEL AND EQUIPMENT WILL NOT BE ALLOWED WITHIN THE AIRPORT OPERATIONS AREA (AOA) UNTIL THE AREA HAS BEEN CLOSED TO AIRCRAFT AND NOTAMS ISSUED.
- ALL WORK WILL BE ACCOMPLISHED DURING NORMAL DAYTIME WORKING HOURS ONLY, OR AS APPROVED BY THE OWNER.
- REFERENCE AC 150/5370-26, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" FOR ADDITIONAL SAFETY INFORMATION AND REQUIREMENTS.
- GENERAL SAFETY REQUIREMENTS:** DURING PERFORMANCE OF THIS CONTRACT, THE AIRPORT RUNWAYS, TAXIWAYS, AND AIRCRAFT PARKING APRONS SHALL REMAIN IN USE BY AIRCRAFT TO THE MAXIMUM EXTENT POSSIBLE. AIRCRAFT USE OF AREAS NEAR THE CONTRACTOR'S WORK WILL BE CONTROLLED TO MINIMIZE DISTURBANCE TO THE CONTRACTOR'S OPERATION. THE CONTRACTOR SHALL NOT ALLOW HIS/HER EMPLOYEES, SUBCONTRACTOR'S, SUPPLIERS, OR ANY PERSON OVER WHO HE/SHE HAS CONTROL TO ENTER OR REMAIN IN ANY PART OF THE AIRPORT WHICH WOULD BE HAZARDOUS TO PERSONS OR TO AIRCRAFT OPERATIONS. WHENEVER AIRCRAFT OPERATIONS REQUIRE, THE ENGINEER, MAY ORDER THE CONTRACTOR TO SUSPEND OPERATIONS, MOVE PLANT, PERSONNEL, EQUIPMENT, AND MATERIALS TO A SAFE LOCATION AND STAND BY UNTIL AIRCRAFT USE IS COMPLETED.
- OBSTACLE FREE ZONE:** CONSTRUCTION ACTIVITY WITHIN AN OBSTACLE FREE ZONE WILL REQUIRE CLOSING PART OR ALL OF THE AFFECTED RUNWAY. SAFETY DIMENSIONS CAN BE FOUND IN AC 150/5370-26.
- APPROACH CLEARANCE TO RUNWAYS:** RUNWAY LANDING THRESHOLDS SHALL BE LOCATED TO PROVIDE AN UNOBSTRUCTIBLE APPROACH SURFACE WITH AN APPROACH RATION OVER EQUIPMENT AND MATERIALS.
- RUNWAY AND TAXIWAY SAFETY AREAS:** CONTRACTOR SHALL COORDINATE WITH THE RESIDENT PROJECT REPRESENTATIVE AND OWNER. CONSTRUCTION ACTIVITY WITHIN A RUNWAY SAFETY AREA WILL REQUIRE CLOSING PART OR ALL OF THE AFFECTED RUNWAY. CONSTRUCTION ACTIVITY WITHIN TAXIWAY SAFETY AREAS/OBSTACLE FREE AREAS IS PERMISSIBLE WHEN THE TAXIWAY IS OPEN TO AIRCRAFT TRAFFIC IF:
 - ADEQUATE WINGTIP/EMPENNAGE CLEARANCE EXIST BETWEEN THE AIRCRAFT AND EQUIPMENT/MATERIAL.
 - EXCAVATIONS, TRENCHES, OR OTHER CONDITIONS ARE CONSPICUOUSLY MARKED AND LIGHTED.
 - NOTICES TO AIRMEN ARE IN EFFECT CONCERNING THE ACTIVITY
- VEHICLE IDENTIFICATION AND PARKING:**
 - AIRCRAFT VEHICLES SHALL BE MARKED WITH COMPANY NAME AND EQUIPMENT SHALL BE IDENTIFIED BY 3"x3" ORANGE AND WHITE CHECKERED FLAGS AND AMBER FLASHING LIGHTS.
 - PRIVATELY OWNED VEHICLE (POV) PARKING SHALL BE AS DESIGNATED BY THE OWNER.
- NO TRENCHES IN OR DIRECTLY ADJACENT TO OPERATIONAL PAVEMENT SHALL REMAIN OPEN OVERNIGHT OR WHEN THE CONTRACTOR FINISHES WORK FOR THE DAY IN THE AREA. TRENCHES NOT BACKFILLED SHALL BE COVERED WITH STEEL PLATES TO ALLOW SAFE PASSAGE BY AIRCRAFT ACROSS THE TRENCH, IF APPROVED BY THE OWNER.
- THE CONTRACTOR SHALL SUPPLY PORTABLE HAND-HELD RADIOS (AVIATION BAND), SET TO A PREDETERMINED FREQUENCY ESTABLISHED BY THE OWNER, TO EACH FLAGMAN, SUPERVISORY INDIVIDUAL, AND ENGINEER'S RESIDENT PROJECT REPRESENTATIVE SO THAT THEY MAY KEEP IN CONSTANT CONTACT AT ALL TIMES WITH THE OWNER. UPON COMPLETION OF THE PROJECT AND ACCEPTANCE BY THE OWNER, THE RESIDENT PROJECT REPRESENTATIVE SHALL RETURN THE RADIO SET TO THE CONTRACTOR. CONTRACTOR SHALL HAVE TWO (2) RADIOS IN CONTACT WITH THE TOWER AT ALL TIMES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL PORTABLE HAND-HELD RADIO SETS IN WORKING ORDER AT ALL TIMES FOR THE DURATION OF THE PROJECT.
- CONSTRUCTION EQUIPMENT AND VEHICLES SHALL NOT EXCEED 15 MPH WITHIN THE AIRPORT PROPERTY.
- THE CONTRACTOR SHALL NOT, FOR ANY REASON ENTER ACTIVE AIRFIELD AREAS WITHOUT EXPLICIT APPROVAL BY THE ENGINEER OR RESIDENT PROJECT REPRESENTATIVE AND WITHOUT PROPER BADGING AND AIRPORT DRIVER TRAINING UNLESS ESCORTED BY BADGED AND TRAINED PERSONNEL. THE CONTRACTOR SHALL LIMIT OPERATIONS TO LIMITS OF CONSTRUCTION ACTIVITIES.
- ALL EQUIPMENT AND MATERIALS SHALL BE STORED IN THE CONTRACTORS STAGING AND STORAGE AREA WHEN NOT IN USE. EQUIPMENT AND MATERIALS SHALL NOT ENCRGOACH UPON ACTIVE OPERATIONAL AREAS. MINIMUM CLEARANCES SHALL BE MAINTAINED ON RUNWAYS IN ACCORDANCE WITH PART 77 OF FEDERAL AVIATION REGULATIONS.
- ALL WASTE OR SPOIL MATERIAL SHALL BE DISPOSED OF OFF THE AIRPORT PROPERTY BY THE CONTRACTOR, UNLESS OTHERWISE NOTED. WASTE AND LOOSE MATERIAL CAPABLE OF DAMAGE TO AIRPLANES MUST NOT BE PLACED ON ACTIVE AIRPLANE MOVEMENT AREAS. MATERIAL TRACKED ON THESE AREAS MUST BE REMOVED CONTINUOUSLY BY A SWEeper OR VACUUM TRUCK SUPPLIED BY THE CONTRACTOR DURING THE PROJECT. NO DIRECT PAY IN BID PROPOSAL.

CONSTRUCTION NOTES

- EACH CONSTRUCTION AREA SHALL BE SHAPED TO ALLOW DRAINAGE OF SURFACE WATER DURING EACH WORK OPERATION. IF NECESSARY, SURFACE WATER SHALL BE PUMPED IMMEDIATELY FROM EACH CONSTRUCTION AREA AFTER EACH RAIN.
- STOCKPILE LOCATION SHALL BE ADJUSTED IN THE FIELD AND COORDINATED WITH THE OWNER AND THE ENGINEER. STOCKPILE SHALL NOT CREATE ANY PONDING OF WATER OR ALTER DRAINAGE PATTERNS OF THE AIRPORT PROPERTY.
- CONTRACTOR SHALL PROVIDE AND INSTALL ALL MATERIALS AND DRAINAGE STRUCTURES TO CONSTRUCT A STABILIZED CONSTRUCTION ENTRANCE/EXIT AT EACH POINT WHERE CONSTRUCTION CREWS WILL EXIT AIRPORT PROPERTY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO EXISTING UNDERGROUND CABLES, PIPELINES, ETC. ENCOUNTERED DURING CONSTRUCTION. ALL REPAIRS MUST BE MADE IN COMPLIANCE WITH THE OWNER AND THE ENGINEER'S REQUIREMENTS. NO DIGGING SHALL BE PERFORMED WITHOUT FIRST CONTACTING THE OWNER. ANY DAMAGE TO

FAA CABLE OR OTHER UNDERGROUND CABLING OR FACILITIES SHALL BE REPAIRED IN ACCORDANCE WITH THE APPLICABLE FAA SPECIFICATIONS AND IN A MANNER ACCEPTABLE TO THE OWNER AND THE ENGINEER AT THE CONTRACTOR'S EXPENSE.

- DAMAGE TO EXISTING PAVEMENTS DUE TO MOVING OR USAGE OF HEAVY EQUIPMENT OR THE TRANSPORT OF MATERIALS TO OR ON THE SITE SHALL BE REPAIRED TO EQUAL OR BETTER QUALITY BY THE CONTRACTOR AT HIS OWN EXPENSE. CONTRACTOR SHALL ALSO MAINTAIN AND REPAIR DAMAGE TO PUBLIC USE ROADS. MAINTENANCE SHALL BE PERFORMED THROUGHOUT THE PROJECT.
- CONSTRUCTION EQUIPMENT AND VEHICLES SHALL TRAVEL A MINIMUM AMOUNT ON NEWLY CONSTRUCTED PAVEMENTS SO THAT THE NEWLY CONSTRUCTED PAVEMENT AREAS WILL NOT BE DAMAGED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO THE NEWLY CONSTRUCTED PAVEMENTS AT THE CONTRACTORS EXPENSE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND ACQUIRING AN ACCEPTABLE WATER SOURCE AS REQUIRED FOR CONSTRUCTION.
- THE CONTRACTOR SHALL ANTICIPATE ALL UNDERGROUND OBSTRUCTIONS SUCH AS, BUT NOT LIMITED TO, WATER MAINS, GAS LINES, STORM AND SANITARY SEWERS, TELEPHONE OR ELECTRIC LIGHT OR POWER DUCTS, CONCRETE, AND DEBRIS. ANY SUCH LINES OR OBSTRUCTIONS INDICATED ON THE DRAWINGS SHOW ONLY THE APPROXIMATE LOCATIONS AND SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR. THE OWNER AND ENGINEER WILL ENDEAVOR TO FAMILIARIZE THE CONTRACTOR WITH ALL KNOWN UTILITIES AND OBSTRUCTIONS, BUT THIS SHALL NOT RELIEVE THE CONTRACTOR FROM FULL RESPONSIBILITY IN ANTICIPATING ALL UNDERGROUND OBSTRUCTIONS WHETHER OR NOT SHOWN ON THE DRAWINGS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO EXISTING FACILITIES NOT DESIGNATED FOR RECONSTRUCTION OR REPLACEMENT, AT CONTRACTORS EXPENSE.
- THE CONTRACTOR SHALL, AT HIS OWN EXPENSE, MAINTAIN IN PROPER WORKING ORDER AND WITHOUT INTERRUPTION OF SERVICE ALL EXISTING UTILITIES AND SERVICES WHICH MAY BE ENCOUNTERED IN THE WORK. WITH THE CONSENT OF THE ENGINEER AND UTILITY OWNER SUCH SERVICE CONNECTIONS MAY BE TEMPORARILY INTERRUPTED TO PERMIT THE CONTRACTOR TO REMOVE DESIGNATED LINES OR TO MAKE TEMPORARY CHANGES IN THE LOCATIONS OF SERVICES. THE COST OF MAKING ANY CHANGES SHALL BE AT THE CONTRACTOR'S EXPENSE.
- NOTIFY ALL UTILITY COMPANIES INVOLVED TO HAVE THEIR UTILITIES LOCATED AND MARKED IN THE FIELD. ALL UNDERGROUND UTILITIES SHALL THEN BE UNCOVERED TO VERIFY LOCATION AND ELEVATION BEFORE CONSTRUCTION BEGINS. COORDINATE WITH UTILITY OWNER IF UTILITY INSPECTOR MUST BE ON SITE WHEN LOCATING OR EXCAVATING NEAR UTILITIES.
- SHEETING AND BRACING:** INSTALL SHEETING AND BRACING NECESSARY TO SUPPORT THE SIDES OF TRENCHES AND OTHER EXCAVATIONS WITH VERTICAL SIDES, AS REQUIRED BY CURRENT OSHA REGULATIONS. COST OF SHEETING AND BRACING SHALL BE SUBSIDIARY TO THE CONSTRUCTION.
- CONSTRUCTION SITE ACCESS AND HAUL ROADS:** ACCESS TO THE JOB SITE SHALL BE AS SHOWN ON THE PLANS OR AS DESIGNATED BY THE OWNER.
- WATER IN EXCAVATION:** KEEP WORK FREE FROM GROUND OR SURFACE WATER AT ALL TIMES. PROVIDE PUMPS OF ADEQUATE CAPACITY OR OTHER APPROVED METHOD TO REMOVE WATER FROM THE EXCAVATION IN SUCH A MANNER THAT IT WILL NOT INTERFERE WITH THE PROGRESS OF THE WORK OR THE PROPER PLACING OF OTHER WORK. THE COST OF DEWATERING THE EXCAVATION SHALL BE SUBSIDIARY TO CONSTRUCTION.
- THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL HORIZONTAL AND VERTICAL CONSTRUCTION STAKING AS REQUIRED FOR THE PROJECT DEVELOPMENT.
- THE EXACT LOCATION(S) OF FILTER FABRIC FENCE SHALL BE DETERMINED IN ACCORDANCE WITH THE PROJECT EROSION CONTROL PLAN. THE FILTER FABRIC FENCE SHALL BE INSTALLED AND MAINTAINED AS SPECIFIED. ANY LOCATIONS OF THE FILTER FABRIC FENCE SHOWN ON THE PLANS ARE APPROXIMATE ONLY. PAYMENT SHALL BE MADE BASED ON THE PRICE AS LISTED IN THE BID PROPOSAL AND THE ORIGINAL LINEAR FEET OF MATERIAL INSTALLED. PAYMENT SHALL NOT BE MADE FOR REPLACEMENT FENCE REQUIRED FOR REPAIR OR MAINTENANCE. THE FILTER FABRIC FENCE SHALL BE MAINTAINED IN PROPER CONDITION AT ALL TIMES DURING THE PROJECT. THE COST OF REPAIRING THE FILTER FABRIC FENCE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- TOPSOIL REPLACEMENT IS REQUIRED IN ALL AREAS. TOPSOIL SHALL INCLUDE THE TOP FOUR (4) INCHES OF TRENCH OR EXCAVATION. TOPSOIL SHALL BE KEPT SEPARATE FROM GENERAL EXCAVATED MATERIAL. IN CULTIVATED AREAS ROCK FROM THE EXCAVATION SHALL NOT BE INCLUDED IN THE TOP SIX INCHES OF TOPSOIL BACKFILL. ALL AREAS OF BACKFILL OR EXCAVATION SHALL BE BROUGHT TO WITHIN FOUR (4) INCHES OF FINAL GRADE AND BROUGHT TO GRADE WITH COMPACTED TOPSOIL.
- UPON COMPLETION OF FINAL GRADING, OR AS REQUIRED BY THE SWPPP, ALL AREAS DISTURBED BY CONTRACTOR SHALL BE SEEDED AND FERTILIZED. (CONTRACTOR SHALL INSTALL EROSION CONTROL MATTING AS SHOWN ON THE PLANS.) CONTRACTOR SHALL INSTALL A MINIMUM FOUR (4) FOOT WIDE BONDED FIBER MATRIX MULCH STRIP ADJACENT TO ALL PAVEMENT EDGES AND ALL SLOPES GREATER THAN 5 PERCENT. THE CONTRACTOR SHALL SOD AREAS AS DESIGNATED ON THE PLANS. THE CONTRACTOR SHALL WATER, FERTILIZE, AND MAINTAIN GRASS FOR AT LEAST TWO CUTTINGS, OR AS REQUIRED TO ENSURE GROWTH.
- REMOVE EROSION CONTROL DEVICES AS DIRECTED ONCE GRASS IS ESTABLISHED TO 70% COVERAGE. NO SEPARATE PAY
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING HIS OWN PROJECT OFFICE, TOILETED FACILITIES AND OTHER NECESSARY BUILDINGS OR SHELTERS. THE OWNER WILL NOT PROVIDE FACILITIES TO THE CONTRACTOR DURING CONSTRUCTION. ALL FACILITIES AND SERVICES FOR RPR SHALL BE PROVIDED BY THE CONTRACTOR AND SHALL BE SUBSIDIARY TO THE VARIOUS BID ITEMS ON THE PROJECT. SEE TECHNICAL SPECIFICATIONS ITEM S-1 FOR INFORMATION REGARDING CONTRACTOR FACILITIES.
- THE CONTRACTOR SHALL MAINTAIN A CLEAN SAFE CONSTRUCTION WORK AREA. THE CONTRACTOR SHALL PERFORM CLEANUP OPERATIONS ON A DAILY BASIS.
- THE CONTRACTOR SHALL NOT DEVIATE FROM THE APPROVED CONSTRUCTION PHASING WITHOUT FIRST OBTAINING APPROVAL FROM THE ENGINEER.
- THE CONTRACTOR SHALL HAVE SUFFICIENT EQUIPMENT AND PERSONNEL ON SITE TO ACCOMPLISH EFFICIENT AND PROMPT CONSTRUCTION OF THE VARIOUS WORK ITEMS, INCLUDING WORK ON MORE THAN ONE WORK ITEM SIMULTANEOUSLY.
- ALL ABOVE GROUND AND GROUND LEVEL ELECTRICAL RELATED APPURTENANCES (I.E. RUNWAY LIGHTS, CABLE BOXES, CABLE AND/OR DUCT MARKERS, CONDUIT ETC.) SHALL BE PROTECTED AT ALL TIMES. ANY DAMAGE DONE TO SAID APPURTENANCES BY THE CONTRACTOR SHALL BE REPAIRED TO LIKE QUALITY AT THE CONTRACTORS EXPENSE. THE REPAIRS SHALL BE PERFORMED TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.
- WORK CANNOT COMMENCE UNTIL:
 - SUFFICIENT BARRICADES ARE IN PLACE TO CONFINE THE WORK AREA AND CREATE A BARRIER BETWEEN AIRCRAFT AND VEHICLE MOVEMENT AREAS AND THE CONSTRUCTION AREA.
 - ALL SAFETY EQUIPMENT FOR PERSONNEL AND CONSTRUCTION EQUIPMENT IS IN PLACE AND OPERABLE.
 - A NOTICE TO PROCEED HAS BEEN ISSUED TO THE CONTRACTOR
- ALL MATERIAL SUBMITTALS FOR ITEMS TO BE USED IN CONSTRUCTION OF THE PROJECT SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO COMMENCEMENT OF WORK.
- A COMPLETE PROJECT SCHEDULE SHALL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING AND PRESENTED AND EXPLAINED BY THE CONTRACTOR TO THE MEETING ATTENDEES. RUNWAY SHUTDOWN DATES MAY BE DETERMINED AT THE PRE-CONSTRUCTION MEETING, OR AT A LATER TIME, AS APPROVED BY THE OWNER AND COORDINATED BY THE RESIDENT PROJECT REPRESENTATIVE.
- WEEKLY PROGRESS MEETINGS ARE MANDATORY. THE DAY OF THE WEEK FOR THESE MEETINGS

MAY BE DETERMINED BY AGREEMENT BETWEEN THE OWNER, THE RPR AND THE CONTRACTOR. THESE MEETINGS SHALL BE CONDUCTED BY THE RPR.

- INTERIM PROJECT SCHEDULES SHALL BE PROVIDED ON THE FIRST OF EACH MONTH AND INCLUDE: ORIGINAL BASELINE, UPDATED TO CURRENT CONSTRUCTION ACTIVITY, ADJUSTED CRITICAL PATH AND RUNWAY/TAXIWAY SHUTDOWN DATES TO BE CLEARLY NOTED.
- THE CONTRACTOR IS RESPONSIBLE FOR THE LOCATION AND RESTRICTIONS FOR SERVICING AND MAINTAINING EQUIPMENT AND DISPOSAL OF USED LUBRICANTS, ETC. ACCORDING TO THE PERTINENT LAW AND REGULATIONS.
- IT IS THE INTENT OF THESE PLANS TO MINIMIZE INTERFERENCE TO AIRCRAFT MOVEMENT; THEREFORE, IN UNCLOSED PORTIONS, AIRCRAFT MOVEMENT SHALL HAVE THE RIGHT-OF-WAY.
- THE CONTRACTOR SHALL PROVIDE A MOTORIZED MECHANICAL SWEEPER ON SITE AT ALL TIME DURING THIS PROJECT TO REMOVE "FOREIGN OBJECTS DAMAGE" (FOD) IN AIRCRAFT MOVEMENT AREAS.
- TEMPORARY FENCING WITH GATES SHALL BE PROVIDED BY THE CONTRACTOR. THE CONTRACTOR SHALL KEEP THE CONSTRUCTION SITE SECURE AT ALL TIMES. THE CONTRACTOR SHALL KEEP THE PERIMETER OF THE AIRPORT SECURE AT ALL TIMES. THE CONTRACTOR SHALL GUARD ANY SECTION OF THE PERIMETER FENCE THAT IS LEFT OPEN TO CONSTRUCTION ACTIVITIES WITH A BADGED EMPLOYEE.
- ALL SAW CUTTING SHALL BE SUBSIDIARY TO THE VARIOUS BID ITEMS ON THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO EXISTING FACILITIES NOT DESIGNATED FOR RECONSTRUCTION OR REPLACEMENT, AT HIS EXPENSE.
- THE CONTRACTOR SHALL ENSURE THAT EQUIPMENT IS NOT PARKED BETWEEN THE EXISTING ARFF STATION AND THE AIRPORT OPERATIONS AREA (AOA). CLEAR PASSAGEWAY MUST BE MAINTAINED BETWEEN THE ARFF STATION AND THE AOA AT ALL TIMES.
- CONTRACTOR WILL HAVE PROJECT SUPERVISOR AND ALL LEAD EMPLOYEES BADGED TO ACT AS ESCORT TO CREW. BADGE FEES AND A MANDATORY CLASS AND DRIVER TRAINING AT THE AIRPORT TO DISCUSS TSA SECURITY REQUIREMENTS WILL BE AT THE EXPENSE OF THE CONTRACTOR. PROJECT SUPERVISOR SHALL KEEP EMPLOYEES WITHIN LINE OF SIGHT AT ALL TIMES.
- CONTRACTOR WILL COMPLY WITH ALL SECURITY DIRECTIVES AND PLANS REQUIRED BY THE AIRPORT AND/OR TSA.
- ANY WORK DESCRIBED IN THE PLANS OR SPECIFICATIONS THAT IS NOT PAID FOR DIRECTLY BY A SPECIFIC BID ITEM SHALL BE CONSIDERED SUBSIDIARY TO THE VARIOUS BID ITEMS OF THE PROJECT.

DEMobilIZATION NOTES

- CONDITIONS OF THE PROJECT AREA UPON COMPLETION OF THE JOB SHALL BE GOOD AS OR BETTER THAN THE CONDITION PRIOR TO STARTING WORK, IN ADDITION TO THE WORK ITEMS LISTED.
- THE PROJECT AREA SHALL BE FREE OF ANY CONTRACTOR STOCKPILE MATERIALS UPON COMPLETION OF THE JOB UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- UPON COMPLETION OF THE PROJECT, ALL THE HAUL ROUTES SHALL BE PROPERLY CLEANED TO PREVENT OBSTRUCTION AND/OR CAUSE INCONVENIENCE TO NORMAL REGULAR TRAFFIC. ALL TEMPORARY HAUL ROUTES SHALL BE REMOVED AND BROUGHT BACK TO ORIGINAL CONDITION OR BETTER.
- THE RPR TRAILER, ALL OF THE CONSTRUCTION EQUIPMENT, AND ANY FACILITIES TEMPORARILY PLACED ON SITE FOR THE PROJECT SHALL BE REMOVED FROM THE SITE.
- ANY PROPERTIES BELONGING TO THE AIRPORT SHALL BE RETURNED TO THE OWNER.
- PROPER DRAINAGE (NO LOCALIZED PONDING) SHALL BE MAINTAINED, PRIOR TO, DURING AND AFTER MOBILIZATION.
- DEMobilIZATION SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER.
- DEMobilIZATION SHALL BE DONE IN A MANNER THAT WILL NOT CAUSE ANY INCONVENIENCE TO AIRPORT OPERATIONS.
- ANY DAMAGE TO THE AIRPORT PROPERTIES DURING DEMobilIZATION SHALL BE REPAIRED AND PAID FOR AT THE CONTRACTOR'S OWN EXPENSE.
- AIRPORT SAFETY REGULATIONS SHALL BE OBSERVED AT ALL TIMES DURING DEMobilIZATION.

ENVIRONMENTAL NOTES

- IN THE EVENT THAT UNANTICIPATED ARCHEOLOGICAL DEPOSITS ARE ENCOUNTERED DURING CONSTRUCTION, WORK IN THE IMMEDIATE AREA WILL CEASE AND THE ENGINEER WILL CONTACT PROFESSIONAL ARCHEOLOGISTS TO INITIATE POST-REVIEW DISCOVERY PROCEDURES UNDER THE PROVISIONS OF 36 CFR 800.13..
- IN THE EVENT THAT UNANTICIPATED HAZARDOUS MATERIALS ARE ENCOUNTERED DURING CONSTRUCTION, WORK IN THE IMMEDIATE AREA WILL CEASE AND THE ENGINEER WILL BE NOTIFIED.
- HAZARDOUS WASTE: ALL SPILLS MUST BE REPORTED IMMEDIATELY TO THE PROJECT MANAGER.
- EROSION CONTROLS SHALL BE INSPECTED EVERY SEVEN DAYS.
- THE FEDERAL MIGRATORY BIRD TREATY ACT (MBTA) (16 USC §703-711), 50 CFR 10, AND FISH & GAME CODE §3503 §3513, AND §3800, PROTECT MIGRATORY AND NONGAME BIRDS, THEIR OCCUPIED NESTS, AND THEIR EGGS. IN THE EVENT THAT MIGRATORY BIRDS ARE ENCOUNTER ON SITE DURING PROJECT CONSTRUCTION FROM FEBRUARY 15 TO OCTOBER 1, THE SUPERINTENDENT WORKING ON THE PROJECT MUST TAKE REASONABLE CARE TO AVOID IMPACTS TO PROTECTED BIRDS, ACTIVE NESTS, EGGS AND/OR THE YOUNG. A PERSON THAT VIOLATES THAT MBTA MAY BE HELD STRICTLY LIABLE FOR ACTIONS THAT RESULT IN UNPERMITTED TAKE. WHEN MIGRATORY BIRD NESTS ARE DISCOVERED WHICH MAY BE ADVERSELY AFFECTED BY CONSTRUCTION ACTIVITY, OR WHEN A BIRD IS FOUND INJURED OR KILLED AS A RESULT OF CONSTRUCTION ACTIVITY, IMMEDIATELY STOP WORK WITHIN 50 FEET OF THE NEST OR BIRD AND NOTIFY THE RESIDENT PROJECT REPRESENTATIVE/ENGINEER/ AND OWNER.
- ANY LOCATIONS OF THE FILTER FABRIC SILT FENCE SHOWN ON THE PLANS ARE APPROXIMATE ONLY. THE EXACT LOCATION(S) OF FILTER FABRIC FENCE SHALL BE DETERMINED BY THE ENGINEER OF THE ENGINEER'S RESIDENT PROJECT REPRESENTATIVE. THE FILTER FABRIC FENCE SHALL BE INSTALLED AND MAINTAINED AS SPECIFIED. PAYMENT SHALL BE MADE BASED ON THE PRICE AS LISTED IN THE BID PROPOSAL AND THE ORIGINAL LINEAR FEET OF MATERIAL INSTALLED. PAYMENT SHALL NOT BE MADE FOR REPLACEMENT FENCE REQUIRED FOR REPAIR OR MAINTENANCE. THE FILTER FABRIC FENCE SHALL BE MAINTAINED IN PROPER CONDITION AT ALL TIMES DURING THE PROJECT. THE COST OF REPAIRING THE FILTER FABRIC FENCE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. FILTER FABRIC FENCE REMOVAL SHALL BE BY THE CONTRACTOR.

ACCESS ROUTE NOTES

- DAMAGE TO EXISTING PAVEMENTS OR FENCES WHICH IS CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED TO ORIGINAL OR BETTER CONDITION AT NO EXPENSE TO THE OWNER.
- ACCESS ROUTES FOR EACH CONSTRUCTION PHASE SHALL BE COORDINATED WITH THE RESIDENT PROJECT REPRESENTATIVE AND OWNER. ROUTES SHOWN ARE PRELIMINARY ONLY, AND SUBJECT TO APPROVAL AND ADJUSTMENT BY THE OWNER. SEE PHASING PLANS FOR ADDITIONAL ACCESS ROUTE INFORMATION.
- THE PAVEMENT ON THE ACCESS ROUTES MAY NOT SUPPORT CONSTRUCTION TRAFFIC. THE CONTRACTOR SHALL LIMIT LOAD AS NECESSARY TO PREVENT DAMAGE OR SHALL INCLUDE IN HIS BID ADEQUATE BUDGET TO REPAIR DAMAGE TO PAVEMENT.

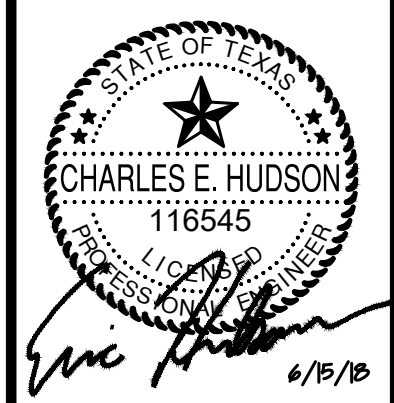
GENERAL NOTES

EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
LONGVIEW, TEXAS

DRAWN BY:	JKP
DESIGNED BY:	CEH
LATEST REVISION:	6/12/2018
ISA JOB NO.:	GC-107

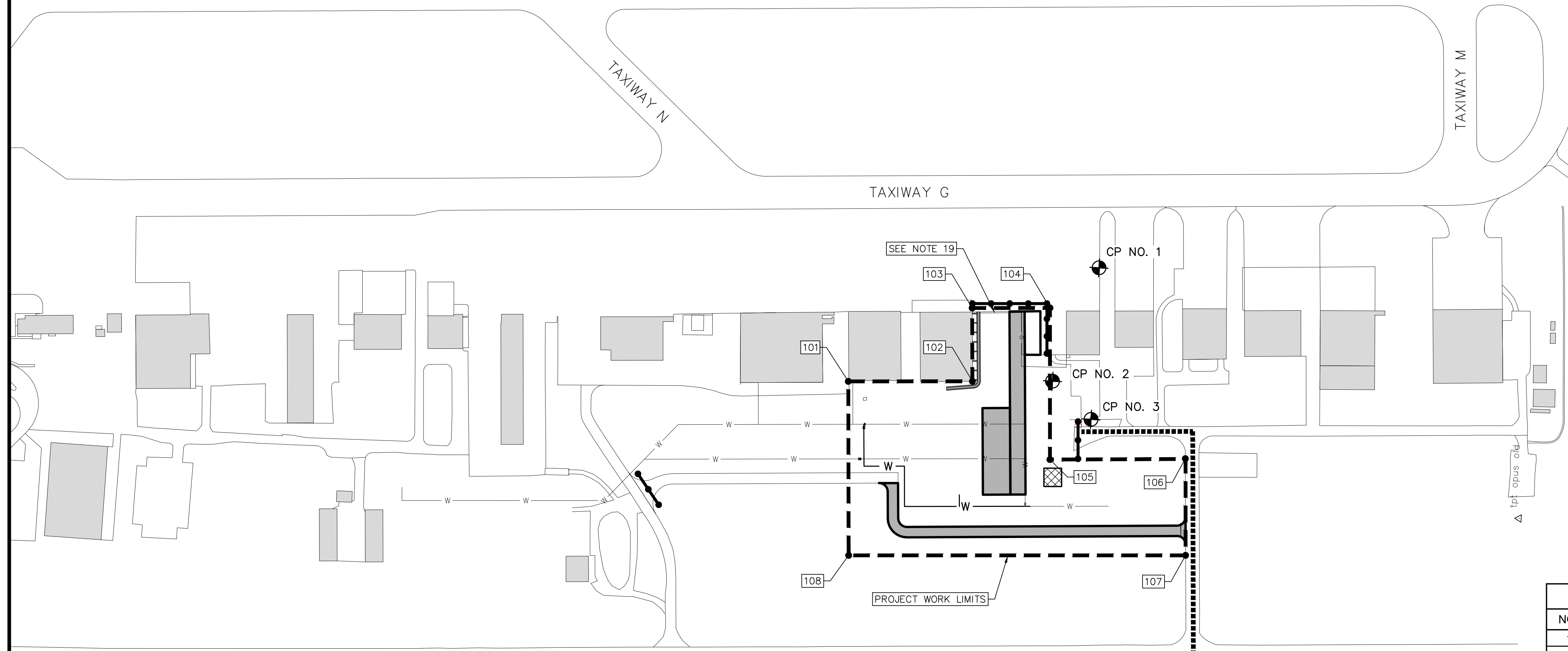
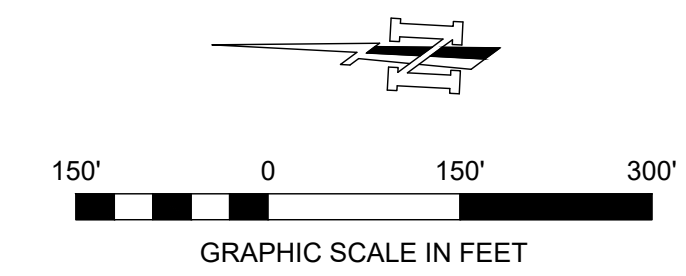


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SEAL:
TBPE Firm Registration No. F-1356
SHEET NO.

RUNWAY 18-36



STATE HIGHWAY 322

CONTROL POINTS				
NO.	DESCRIPTION	NORTHING	EASTING	ELEV
1	1/2" IR	6842273.7590	3135627.0550	366.94
2	1/2" IR WITH CAP	6842567.4410	3135359.9840	368.87
3	1/2" IR	6842273.9150	3135276.6540	369.32

COORDINATE TABLE						
POINT NO.	LATITUDE	LONGITUDE	NORTHING	EASTING	ELEV.	DESCRIPTION
101	N32° 22' 54.45"	W94° 43' 09.94"	6842838.4405	3135334.8969	362.76'	PHASE LIMITS
102	N32° 22' 51.61"	W94° 43' 09.88"	6842551.5080	3135350.1010	363.93'	PHASE LIMITS
103	N32° 22' 51.64"	W94° 43' 07.91"	6842561.5744	3135519.0043	365.28'	PHASE LIMITS
104	N32° 22' 49.87"	W94° 43' 07.87"	6842382.1248	3135528.6013	367.94'	PHASE LIMITS
105	N32° 22' 49.81"	W94° 43' 11.96"	6842363.8248	3135179.0021	368.87'	PHASE LIMITS
106	N32° 22' 46.71"	W94° 43' 11.88"	6842051.0644	3135196.7264	366.07'	PHASE LIMITS
107	N32° 22' 46.88"	W94° 43' 14.48"	6842039.4233	3134974.4531	364.11'	PHASE LIMITS
108	N32° 22' 54.37"	W94° 43' 14.62"	6842816.5662	3134933.7247	370.52'	PHASE LIMITS

PROJECT NOTES:

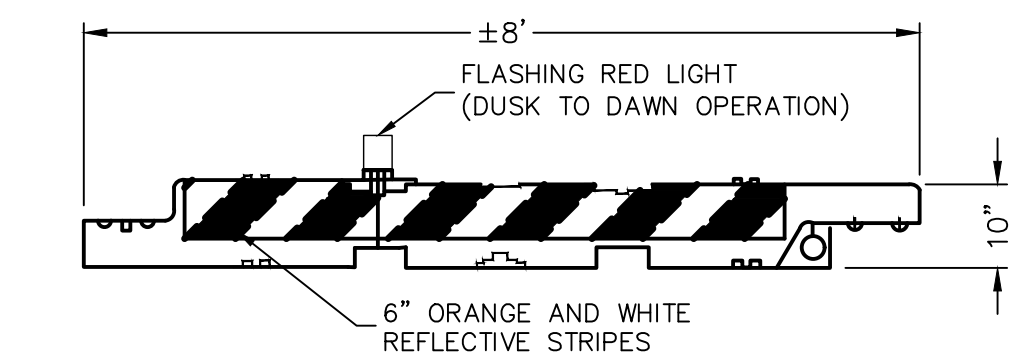
- CONTRACTOR'S CONSTRUCTION ACCESS ROUTE AND STAGING AREAS TO BE USED DURING CONSTRUCTION OF THIS PROJECT ARE TO BE LEFT IN A CONDITION EQUAL TO OR BETTER THAN THE CURRENT CONDITION. ON SITE CONSTRUCTION ACCESS ROUTE IS APPROXIMATE AND WILL BE APPROVED IN THE FIELD BY THE ENGINEER. ANY NECESSARY HAUL ROUTE CROSS DRAINS ARE TO BE INSTALLED AT NO DIRECT PAY. CONSTRUCTION ACCESS ROUTES SHALL BE SEEDED, FERTILIZED AND MULCHED AT NO DIRECT PAY.
- CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY CONSTRUCTION SIGNAGE AS REQUIRED (NO DIRECT PAY).
- BARRICADES SHALL BE PLACED AS DIRECTED BY ENGINEER OR ENGINEER'S REPRESENTATIVE TO ISOLATE WORK AREAS FROM AIRCRAFT.
- CONTRACTOR SHALL REMOVE EXISTING TAXIWAY CENTERLINE RETROREFLECTIVE MARKERS AND DELIVER TO AIRPORT MANAGER.

SAFETY NOTES:

- THE CONTRACTOR SHALL APPLY FAA ADVISORY CIRCULAR 150/5370-2F "OPERATIONAL SAFETY DURING CONSTRUCTION" AS NECESSARY.
- CONTRACTOR SHALL COMPLY WITH THE SAFETY PLAN ASSOCIATED WITH THIS CONSTRUCTION PROJECT AND ENSURE THAT CONSTRUCTION PERSONNEL ARE FAMILIAR WITH SAFETY PROCEDURES AND REGULATIONS ON THE AIRPORT.
- THIS PROJECT INCLUDES WORK WITHIN OR NEAR THE AIR OPERATIONS AREA (AOA). THE AOA IS DEFINED AS ALL RUNWAYS AND THEIR SAFETY AREAS, TAXIWAYS AND THEIR SAFETY AREA AND ALL APRONS. NO MEN OR EQUIPMENT SHOULD BE IN THE RUNWAY SAFETY AREA WHEN THE RUNWAY IS BEING UTILIZED BY AIRCRAFT. IF THE DESIGNATED HAUL ROUTE MUST FALL WITHIN THE RUNWAY SAFETY AREA THE RUNWAY SHALL BE CLOSED WHILE THIS HAUL ROUTE IS IN OPERATION. EACH RUNWAY END IS TO BE MARKED WITH A TEMPORARY "X" WHENEVER THE RUNWAY IS CLOSED FOR CONSTRUCTION.
- CONTRACTOR TO PROVIDE AT LEAST A ONE WEEK NOTICE TO AIRPORT MANGER BEFORE ANY PHASE CHANGES AND CLOSURES. CONTRACTOR SHALL MAINTAIN AIRCRAFT ACCESS TO APRONS AT ALL TIMES OR AS OTHERWISE APPROVED BY THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL WHEN REQUESTED, SUSPEND OPERATIONS IN A SPECIFIC AREA. WORK WILL ONLY RESUME IN THIS AREA WHEN AUTHORIZED BY AIRPORT MANAGER.

- THE CONTRACTOR SHALL KEEP GATES AND FENCES INTACT THROUGHOUT THE DURATION OF THE CONSTRUCTION PROJECT TO PREVENT WILDLIFE FROM ENTERING INTO THE AOA. THE CONTRACTOR PERSONNEL SHALL NOT LEAVE FOOD SCRAPS ON OR NEAR THE AOA.
- NOTAM (NOTICE TO AIRMEN) SHALL BE ISSUED BY THE AIRPORT MANAGER REGARDING MEN OR EQUIPMENT WORKING IN THE AOA UPON ISSUANCE OF A NOTICE TO PROCEED BY THE OWNER. WHEN THE AREA IS HAZARD MARKED AND PROPER NOTAM'S HAVE BEEN ISSUED, SPECIAL CARE MUST BE TAKEN TO INSURE PROPER WING TIP AND PROPELLER CLEARANCE.
- CONTRACTOR SHALL PROVIDE ONE WEEK'S NOTICE PRIOR TO BEGINNING CONSTRUCTION FOR ISSUANCE OF NOTAM BY THE AIRPORT MANAGER.
- ALL VEHICLES OPERATING ON AIRPORT PROPERTY SHALL BE MARKED AND LIGHTED.
- BARRICADES SHALL BE PROVIDED AS SHOWN OR AS DIRECTED BY THE ENGINEER AS WORK PROGRESSES. REQUIRED LOW PROFILE LIGHTED BARRICADES SHALL BE PLACED AROUND AREAS UNDER CONSTRUCTION WHERE PLANES WILL TRAVEL. BARRICADES SHALL BE PROPERLY ANCHORED WITH SAND BAGS OR OTHER MEANS TO ENSURE THAT BARRICADES STAY IN PLACE. LIGHTED BARRICADES SHALL BE CHECKED DAILY. IF THE LIGHTS ARE NOT OPERATING PROPERLY THEY SHALL BE REPAIRED OR REPLACED IMMEDIATELY. BARRICADES SHALL BE UTILIZED UNTIL THE AREA UNDER CONSTRUCTION IS READY FOR OPERATION AS DETERMINED BY THE ENGINEER AND THE AIRPORT MANAGER. SEE SPECIFICATION, GENERAL PROVISIONS SECTION 40, 70 AND 80.
- CONTRACTOR SHALL NOT DEVIATE FROM THE DESIGNATED HAUL ROUTE UNLESS PRIOR APPROVAL IS GRANTED BY THE AIRPORT MANAGER. THE DESIGNATED HAUL ROUTE AND PARKING AREA TO BE USED DURING CONSTRUCTION OF THIS PROJECT ARE TO BE MAINTAINED AND LEFT IN A CONDITION EQUAL TO OR BETTER THAN ITS CURRENT CONDITION.
- STAGING AREA IS LOCATED AS SHOWN. OTHER AREAS MAY BE MADE AVAILABLE BY THE AIRPORT MANAGER AS NECESSARY. NO EQUIPMENT OR BULK MATERIAL SHALL BE PERMITTED IN THE AOA AT THE END OF A WORKING DAY.
- CONTRACTOR PERSONNEL WILL PARK THEIR PERSONAL VEHICLES IN THE STAGING AREA SHOWN. NO PERSONAL VEHICLES WILL BE ALLOWED IN THE (AOA). ALL CONSTRUCTION VEHICLES INSIDE THE AOA WILL BE MARKED WITH ORANGE-AND-WHITE-CHECKERED FLAGS OR FLASHING YELLOW BEACONS WITH COMPANY NAME.
- MATERIAL/EQUIPMENT STORAGE WILL BE LOCATED IN THE DESIGNATED STAGING AREA. ALL MATERIAL SHALL BE STORED IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE MATERIAL/EQUIPMENT STORAGE AREA IN A NEAT AND ORDERLY MANNER.

- THE AIRPORT PROPERTY SHALL REMAIN SECURE THROUGHOUT THE DURATION OF THE CONSTRUCTION PROJECT. ALL FENCES AND GATES SHALL BE LEFT INTACT AT THE END OF THE WORKING DAY.
- CONTRACTOR SHALL KEEP THE WORK LOCATION FREE OF ANY AND ALL DEBRIS (FOD) WHICH MAY CAUSE DAMAGE TO AIRCRAFT. ANY DAMAGE TO AIRCRAFT CAUSED BY CONTRACTOR NEGLIGENCE SHALL BE PAID FOR AT THE FULL EXPENSE OF THE CONTRACTOR. HAUL ROUTES USED FOR ACCESS SHALL BE SWEEPED AND DEBRIS PICKED UP AND REMOVED AS DIRECTED. NO DIRECT PAY. ACTIVE APRONS, TAXIWAYS AND RUNWAYS AFFECTED DURING CONSTRUCTION SHALL BE SWEEPED AND DEBRIS REMOVED FROM THE SURFACE EACH DAY. NO DIRECT PAY. EACH WORK LOCATION SHALL BE INSPECTED BY AIRPORT PERSONNEL PRIOR TO OPENING FOR USE.
- THE CONTRACTOR SHALL PROVIDE A POINT OF CONTACT (POC) FOR THE AIRPORT MANAGER TO BE NOTIFIED OF SAFETY AND SECURITY ISSUES. THE CONTRACTOR SHALL MEET WITH ALL CONTRACTOR AND SUBCONTRACTOR PERSONNEL DAILY AND UPDATE THEM IMMEDIATELY OF NEW ISSUES. A PERMANENT RECORD OF ISSUES SHALL BE KEPT BY THE CONTRACTOR'S POINT OF CONTACT. THIS SHALL BE USED TO INFORM NEW PERSONNEL OF SAFETY AND SECURITY ISSUES.
- RADIO FREQUENCY FOR EAST TEXAS REGIONAL AIRPORT IS CTAF 121.6.
- CONTRACTOR SHALL INSTALL A TEMPORARY FENCE (AT NO DIRECT PAY) TO SECURE THE AIRFIELD PRIOR TO REMOVAL OF THE EXISTING FENCE. NEW FENCE MUST BE INSPECTED BY TSA PRIOR TO REMOVAL OF EXISTING. THE CONTRACTOR MAY ELECT TO LEAVE THE EXISTING FENCE IN PLACE UNTIL THE END OF THE PROJECT OR UNTIL THE NEW FENCE IS COMPLETED AND ACCEPTED.



NOTES

- LIGHTED BARRICADES SHALL BE PROVIDED, INSTALLED, AND MAINTAINED BY CONTRACTOR.
- BARRICADE LIGHTING SHALL BE SPACED NOT TO EXCEED 20' ON CENTER AND SPACING BETWEEN BARRICADES NOT EXCEEDING 10'.
- BARRICADES SHALL BE REQUIRED AROUND ALL EXCAVATIONS WHICH EXCEED 3" IN DEPTH BELOW ADJACENT PAVEMENT AND IN LOCATIONS SHOWN.
- CONTRACTOR SHALL CHECK LAYOUT AND LIGHTS EACH DAY TO ENSURE BARRICADES ARE FUNCTIONING CORRECTLY.

TEMPORARY WATER FILLED LOW-PROFILE BARRICADE
N.T.S.

LEGEND	
	PROPOSED PAVEMENT
	WORK LIMITS
	STAGING AREA
	CONSTRUCTION ACCESS ROUTE
	EXISTING BUILDING
	BARRICADE LOCATION

PROJECT TITLE: SOUTHWEST GA AREA TAXILANE - PHASE I - LONGVIEW, TEXAS

SHEET NAME: GC.107

PROJECT LAYOUT AND SAFETY PLAN

EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
LONGVIEW, TEXAS

MARK

REVISION

DATE

DESIGNED BY: JKP

DESIGNED BY: CEH

LATEST REVISION: 6/8/2018

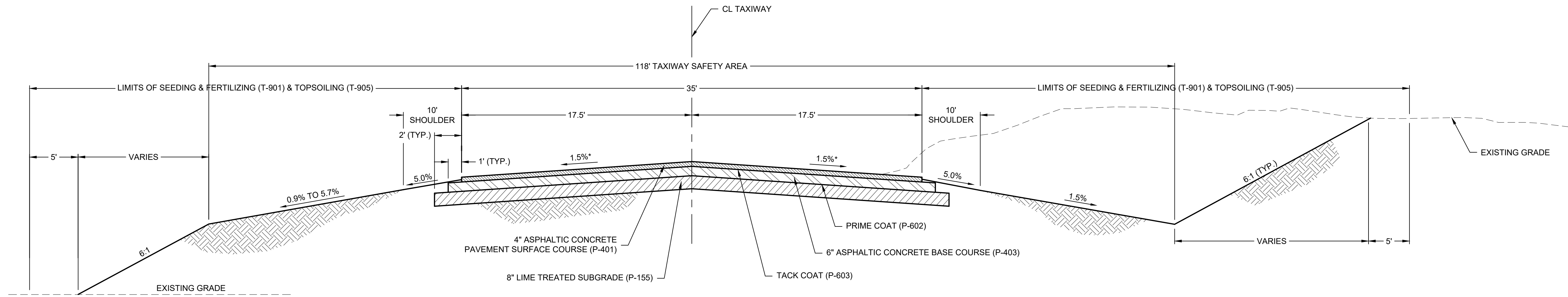
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GC.107

SEAL: TBPE Firm Registration No. F-1356

SHEET NO.

4



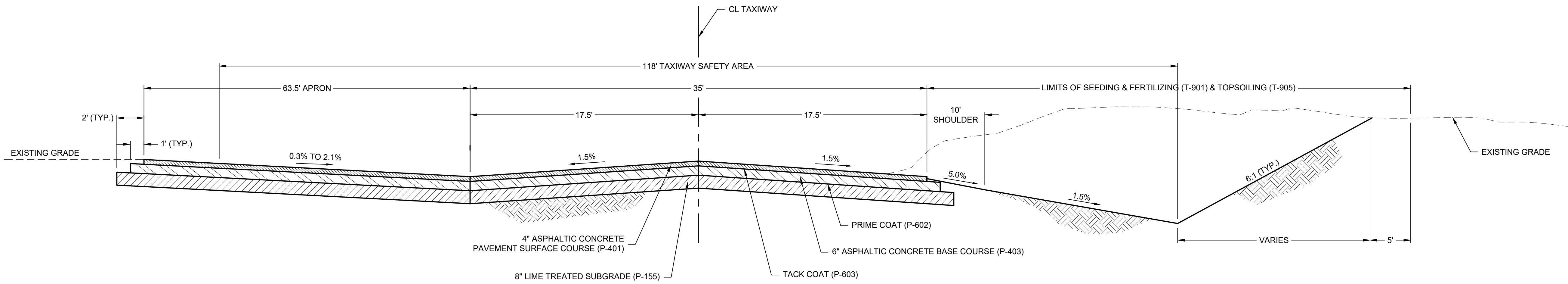
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TYPICAL FINISHED SECTION - ASPHALT

CUT SECTION

N.T.S.

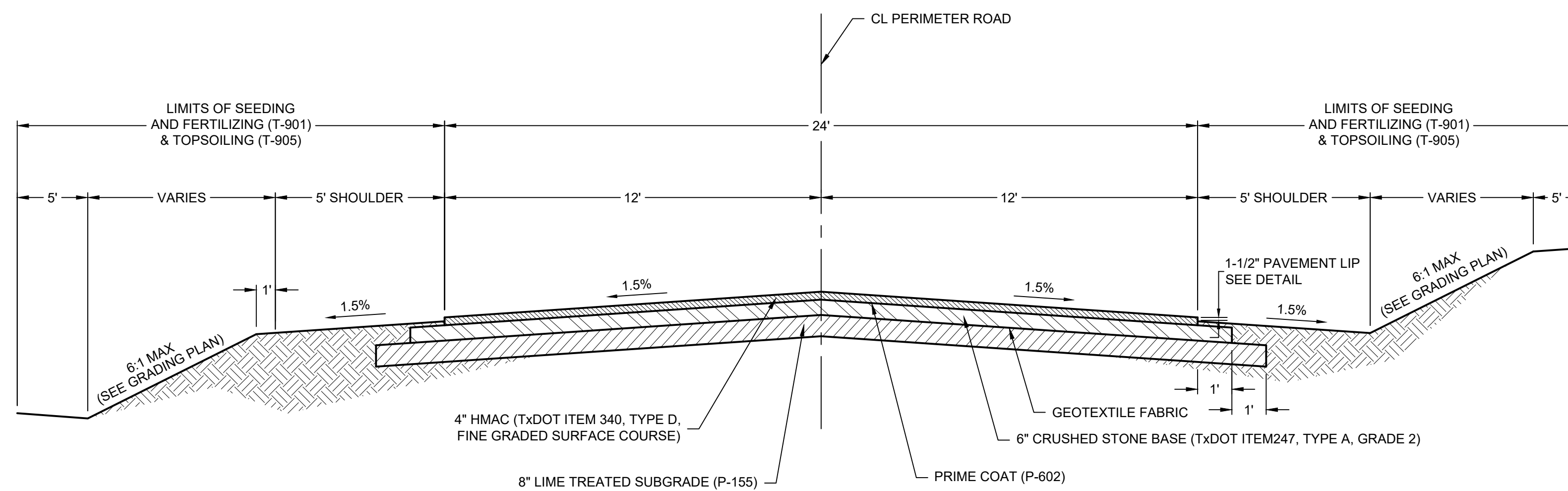
* VARIES STA 5+50 TO STA 6+14.13



TYPICAL FINISHED SECTION AT APRON - ASPHALT

CUT SECTION

N.T.S.



24' PERIMETER MAINTENANCE ROAD - TYPICAL SECTION

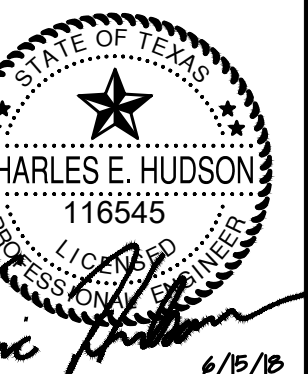
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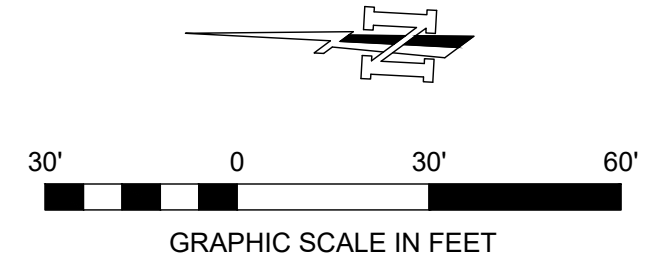
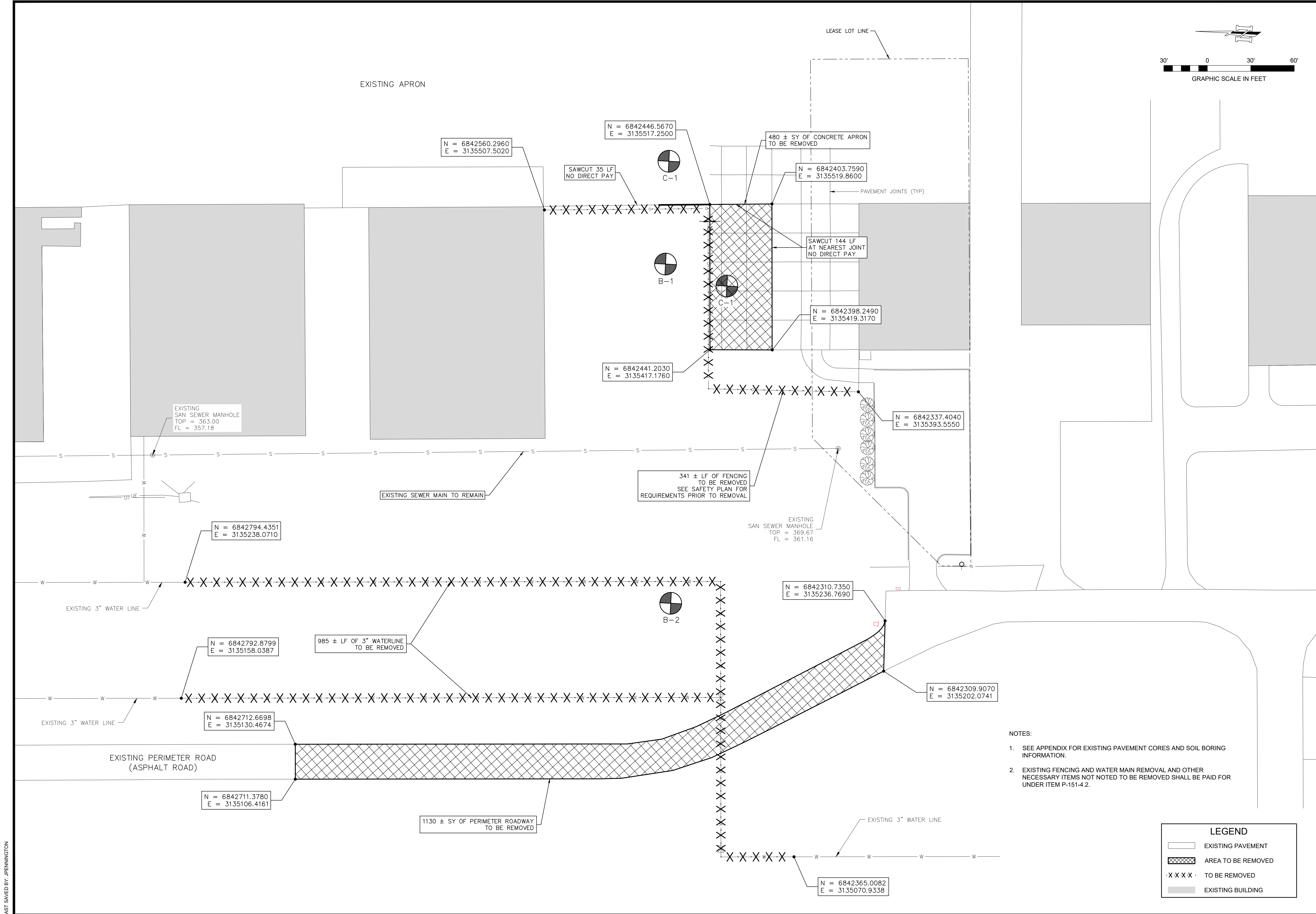
TYPICAL SECTIONS

EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
LONGVIEW, TEXAS

DRAWN BY:	JKP
DESIGNED BY:	CEH
LATEST REVISION:	6/8/2018
ASA JOB NO.:	GC.107



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DEMOLITION PLAN

EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
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ASA JOB NO.:	GC-107

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SHEET NO. **6**

- NOTES:
- SEE APPENDIX FOR EXISTING PAVEMENT CORES AND SOIL BORING INFORMATION.
 - EXISTING FENCING AND WATER MAIN REMOVAL AND OTHER NECESSARY ITEMS NOT NOTED TO BE REMOVED SHALL BE PAID FOR UNDER ITEM P-151-4.2.

LEGEND

	EXISTING PAVEMENT
	AREA TO BE REMOVED
	TO BE REMOVED
	EXISTING BUILDING

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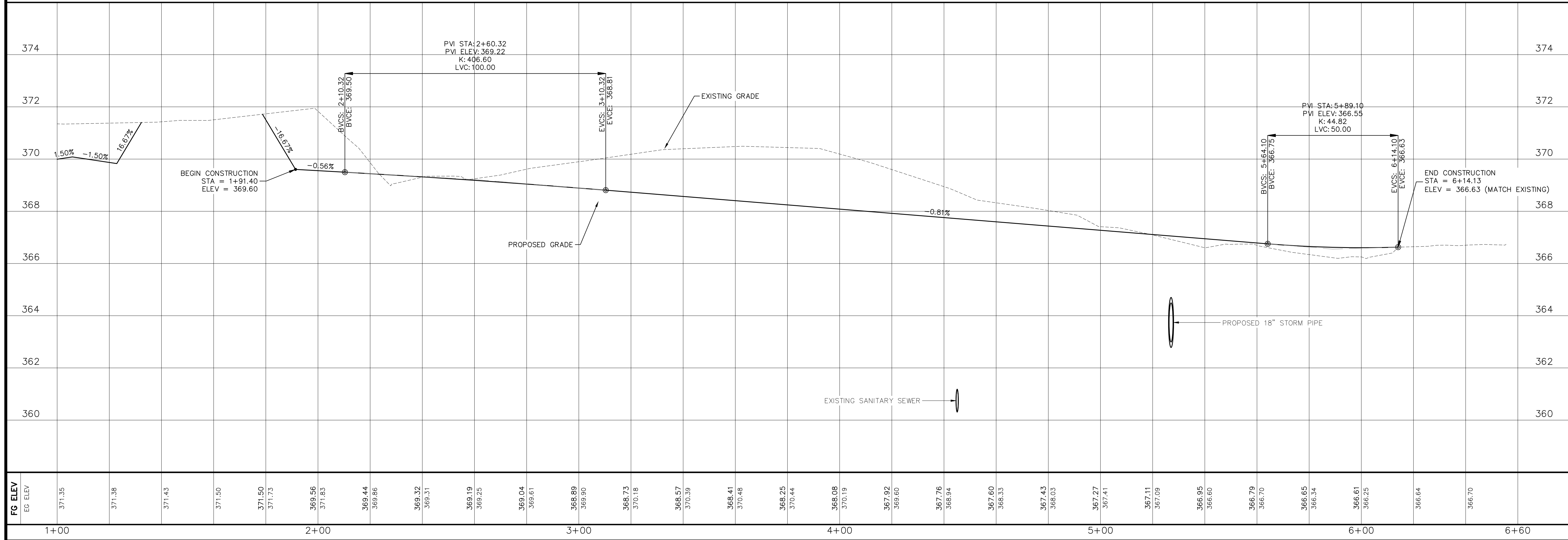
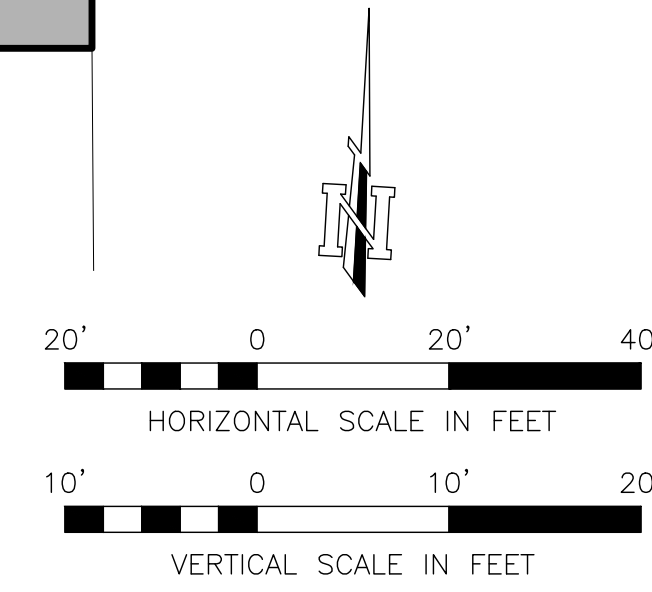
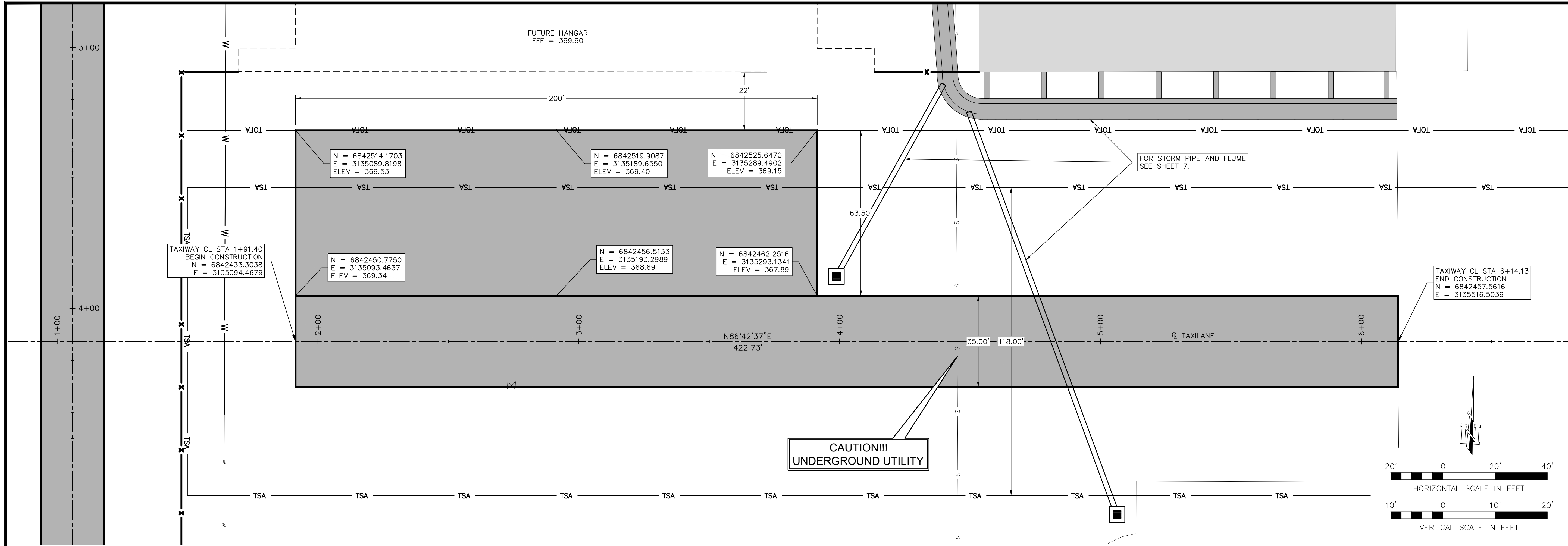
PROJECT TITLE: SOUTHWEST GA AREA TAXILANE - PHASE I - LONGVIEW, TEXAS

SHEET NAME:

PROJECT NAME:

GC-107

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**EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
LONGVIEW, TEXAS**

PROJECT TITLE: SOUTHWEST GA AREA TAXILANE - PHASE I - LONGVIEW, TEXAS

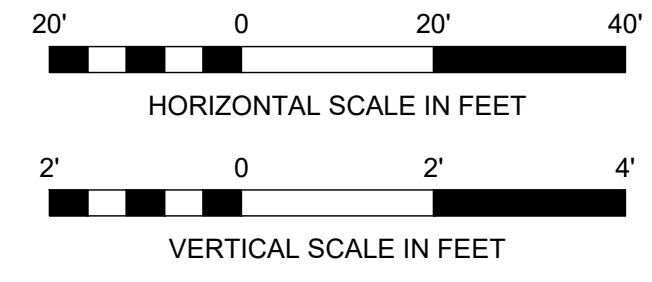
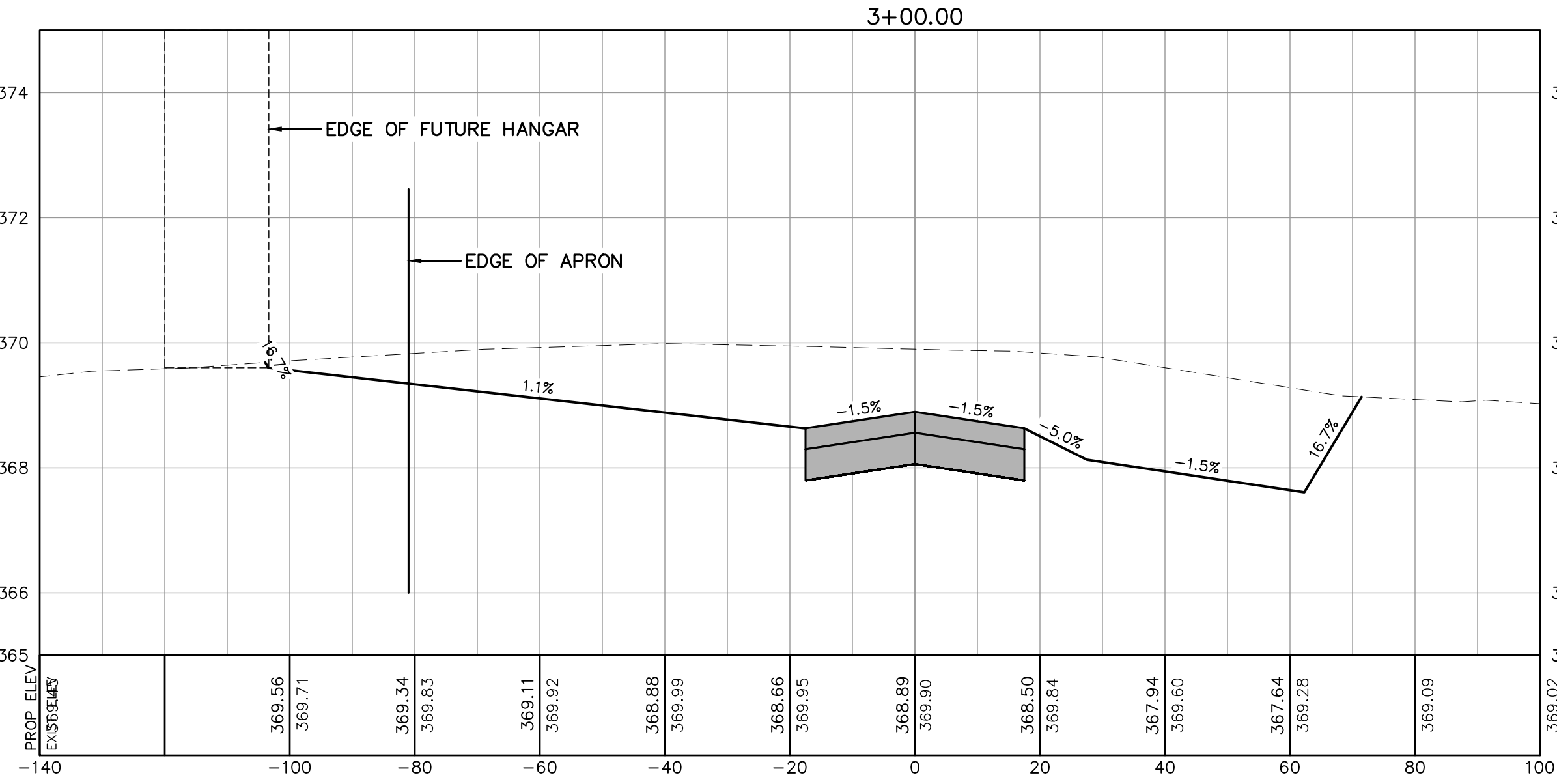
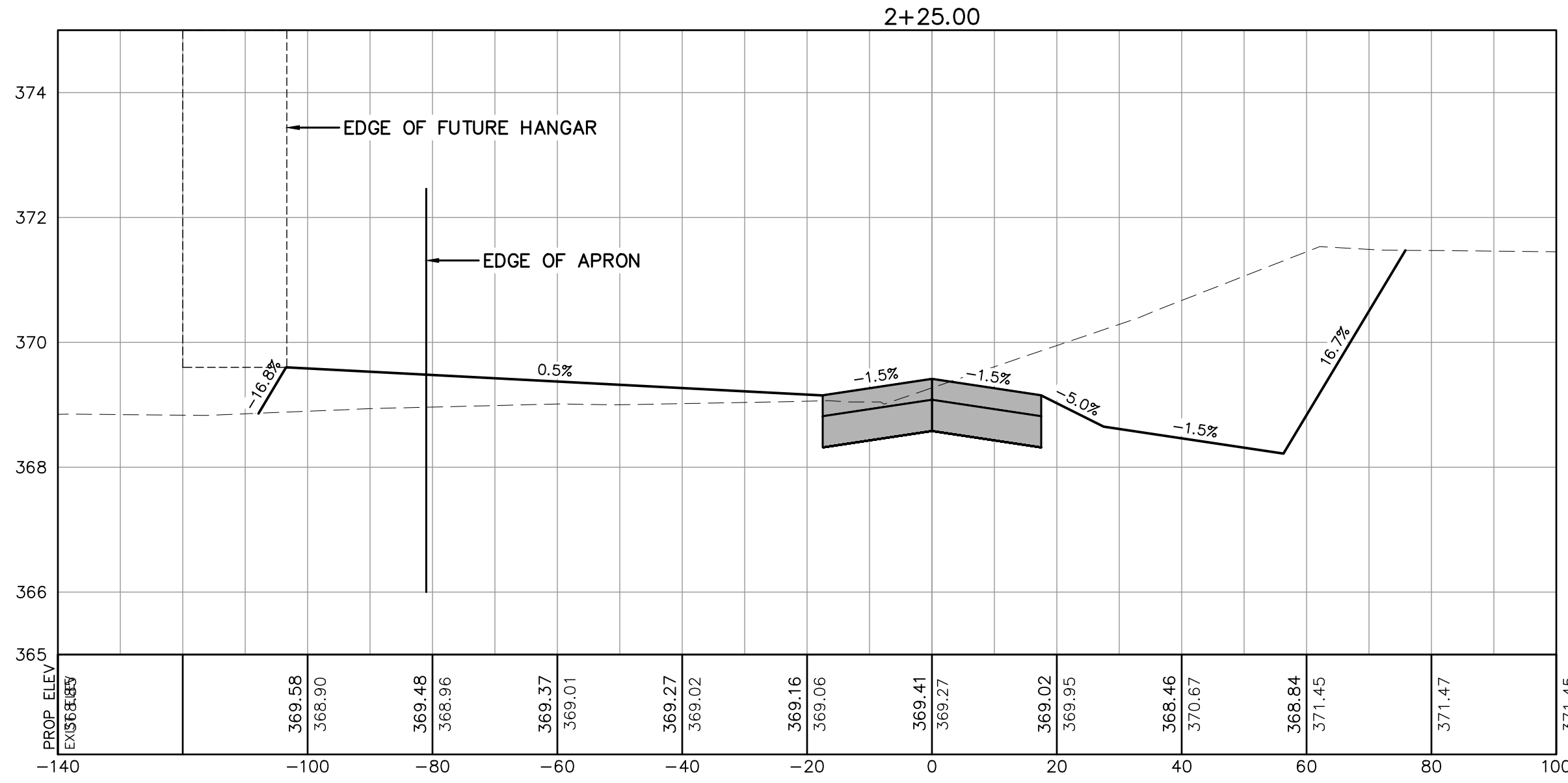
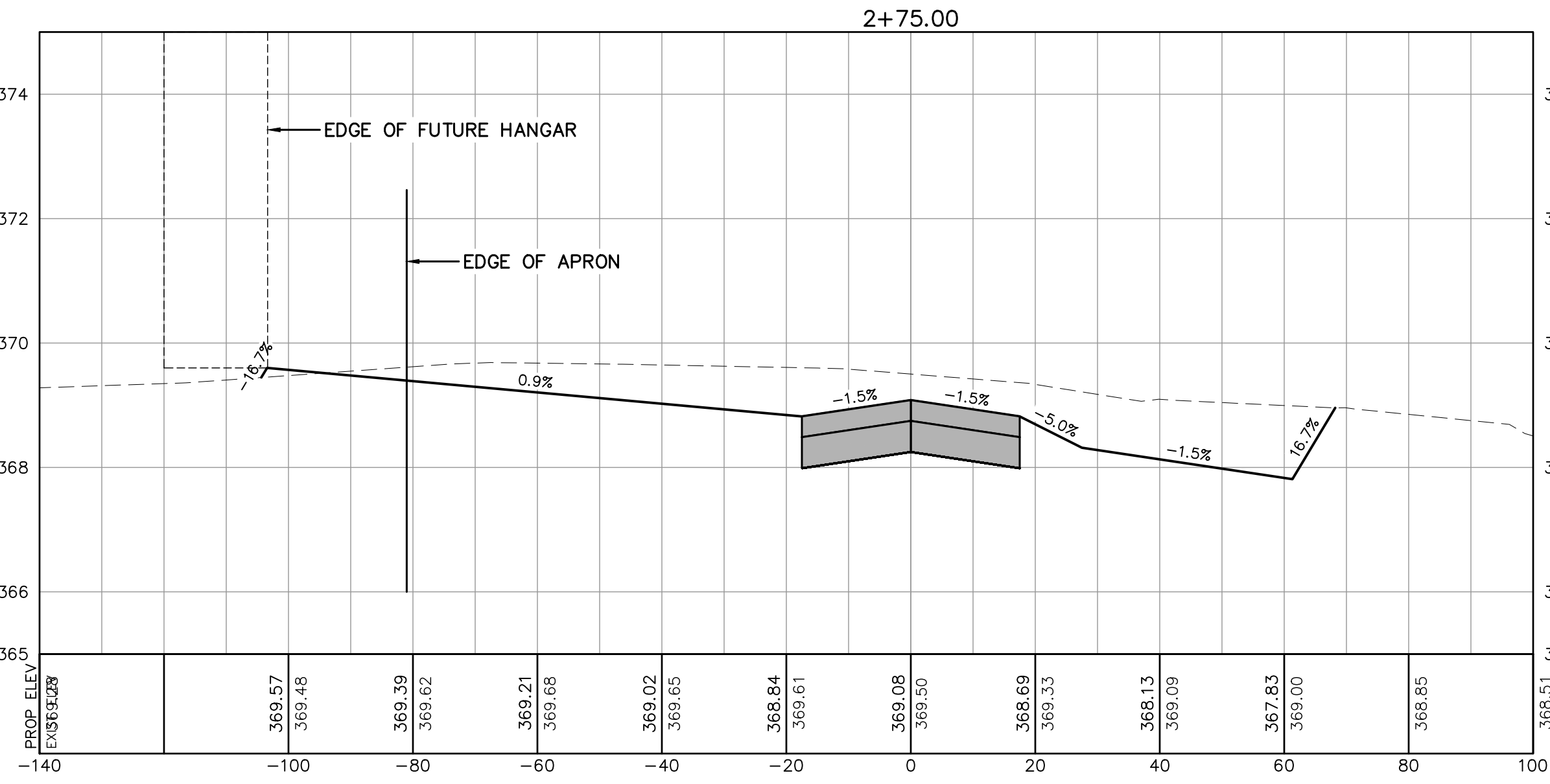
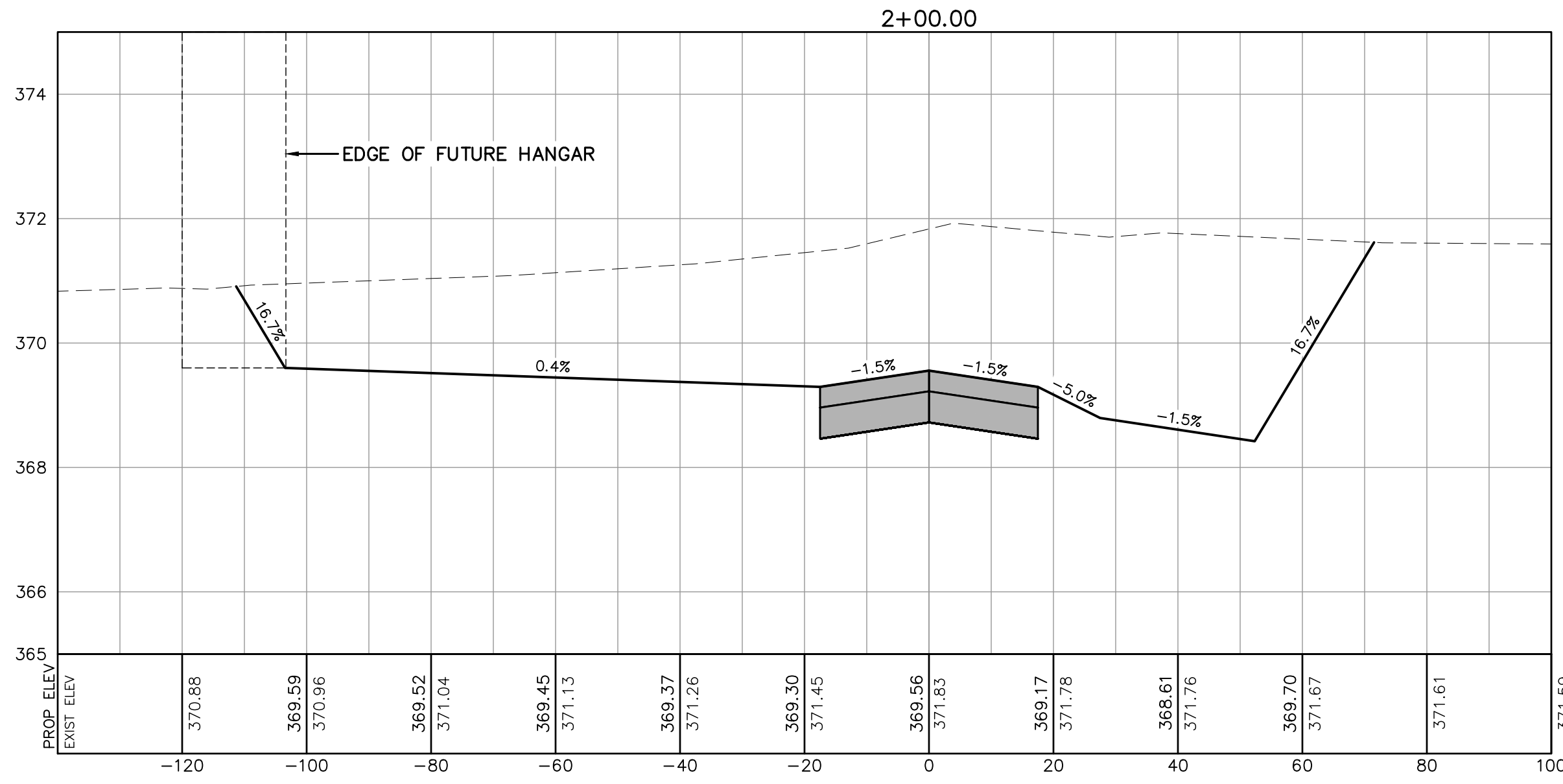
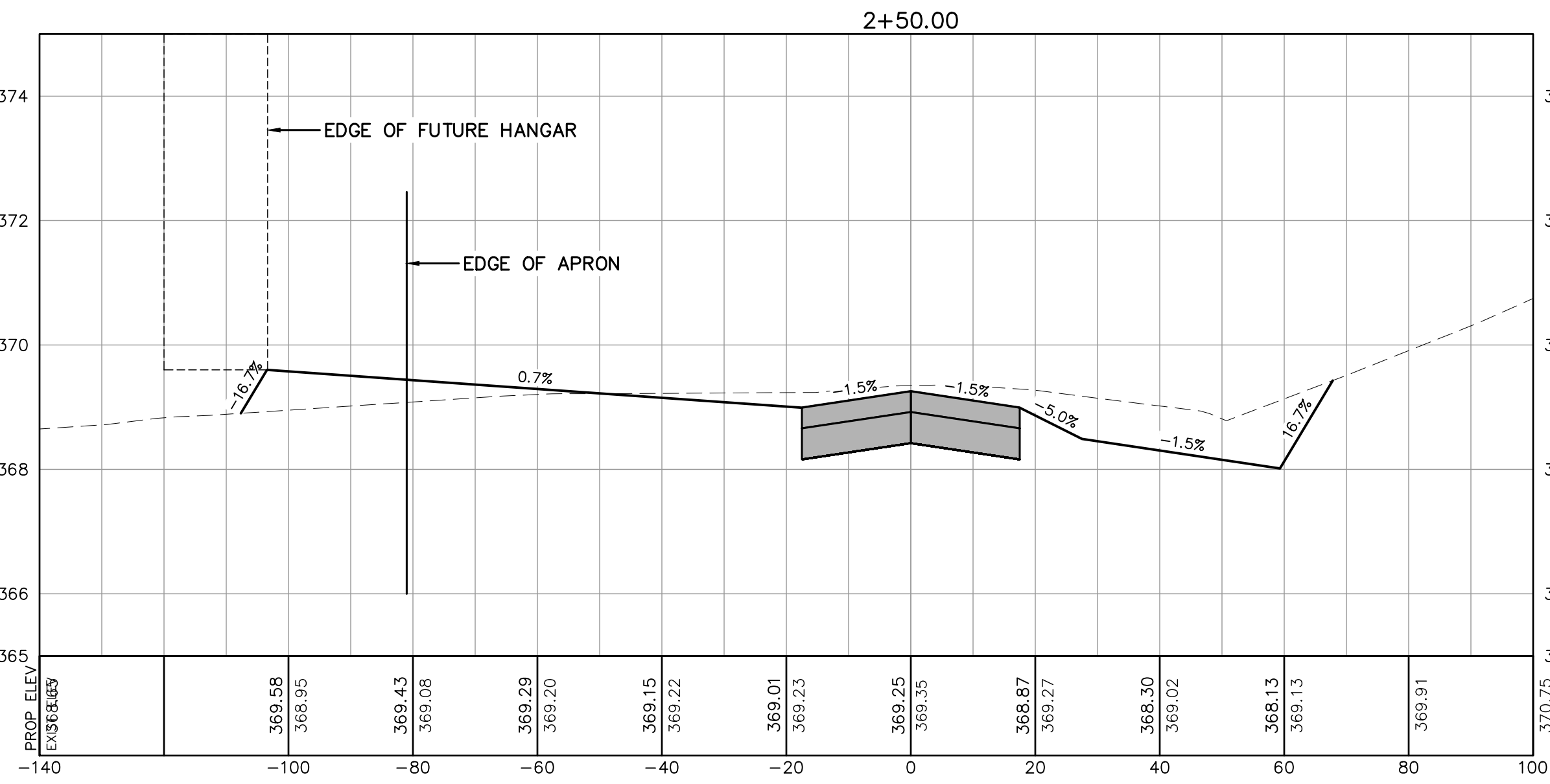
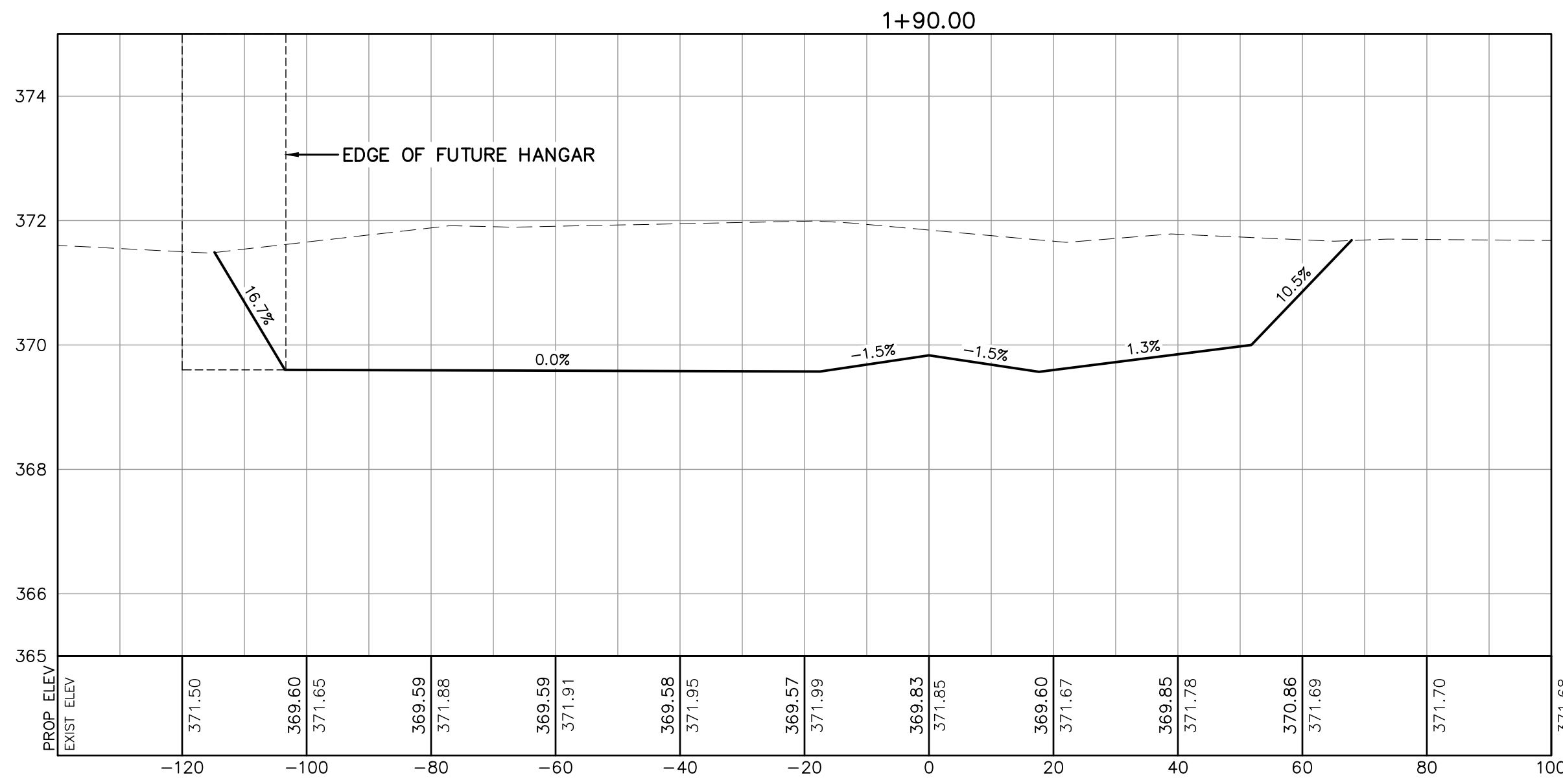
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DESIGNED BY: CEH
LATEST REVISION: 6/8/2018
ISSA JOB NO: KSA-224-9418

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LICENSED PROFESSIONAL ENGINEER

SEAL: TBPE Firm Registration No. F-1356
SHEET NO. 08

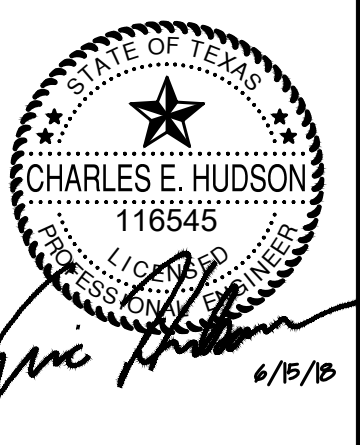


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TAXIWAY CROSS SECTIONS I

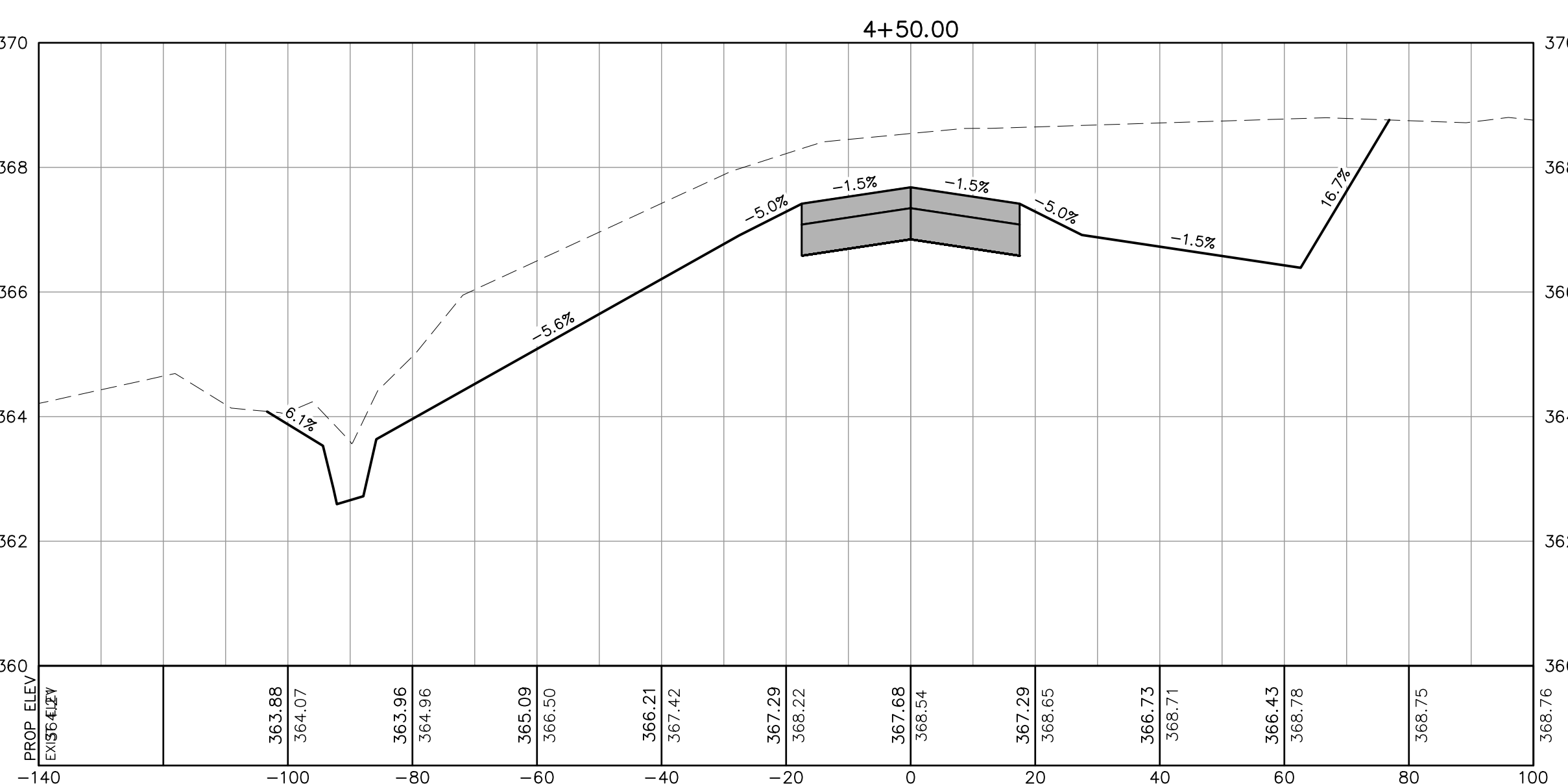
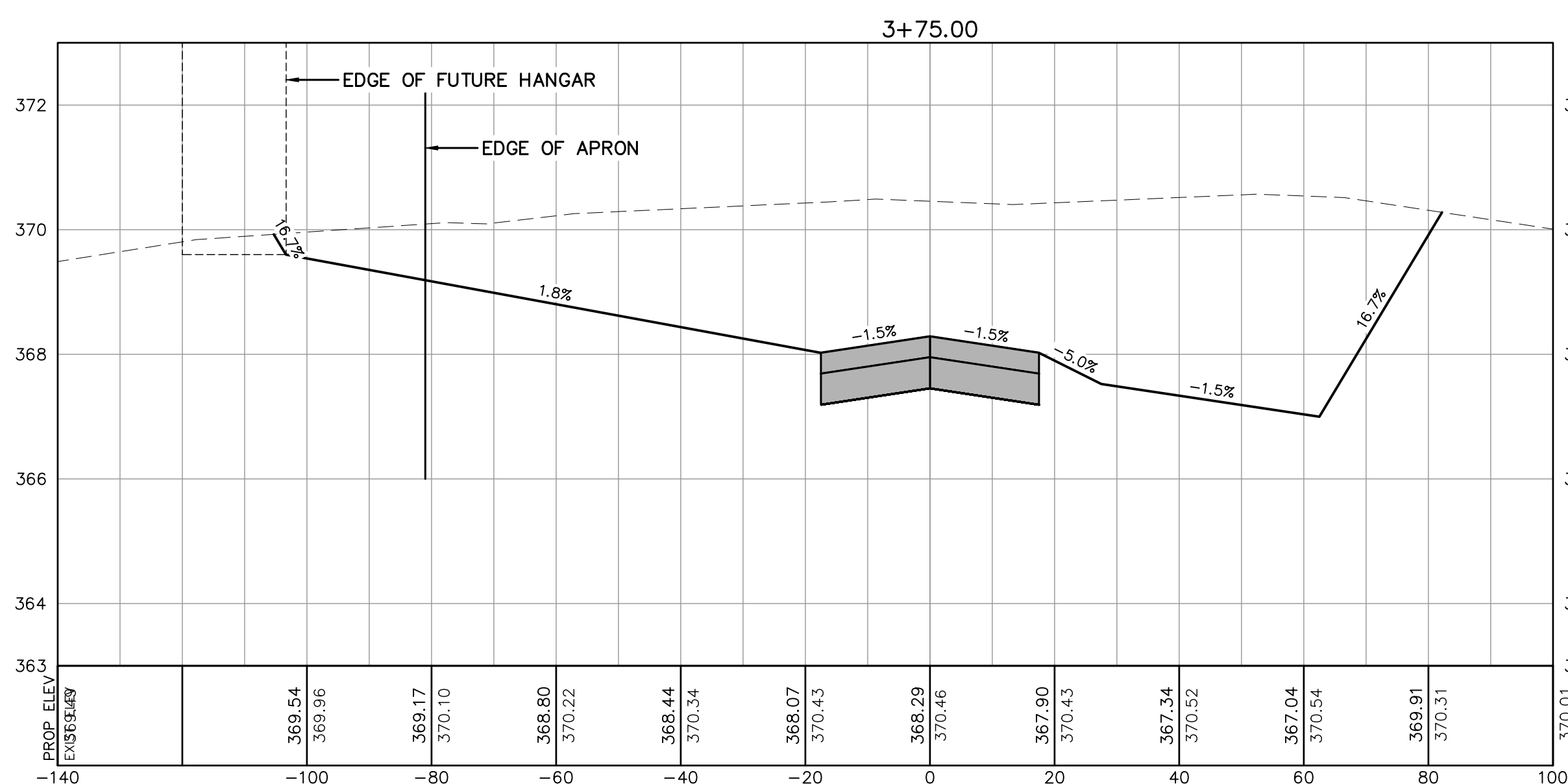
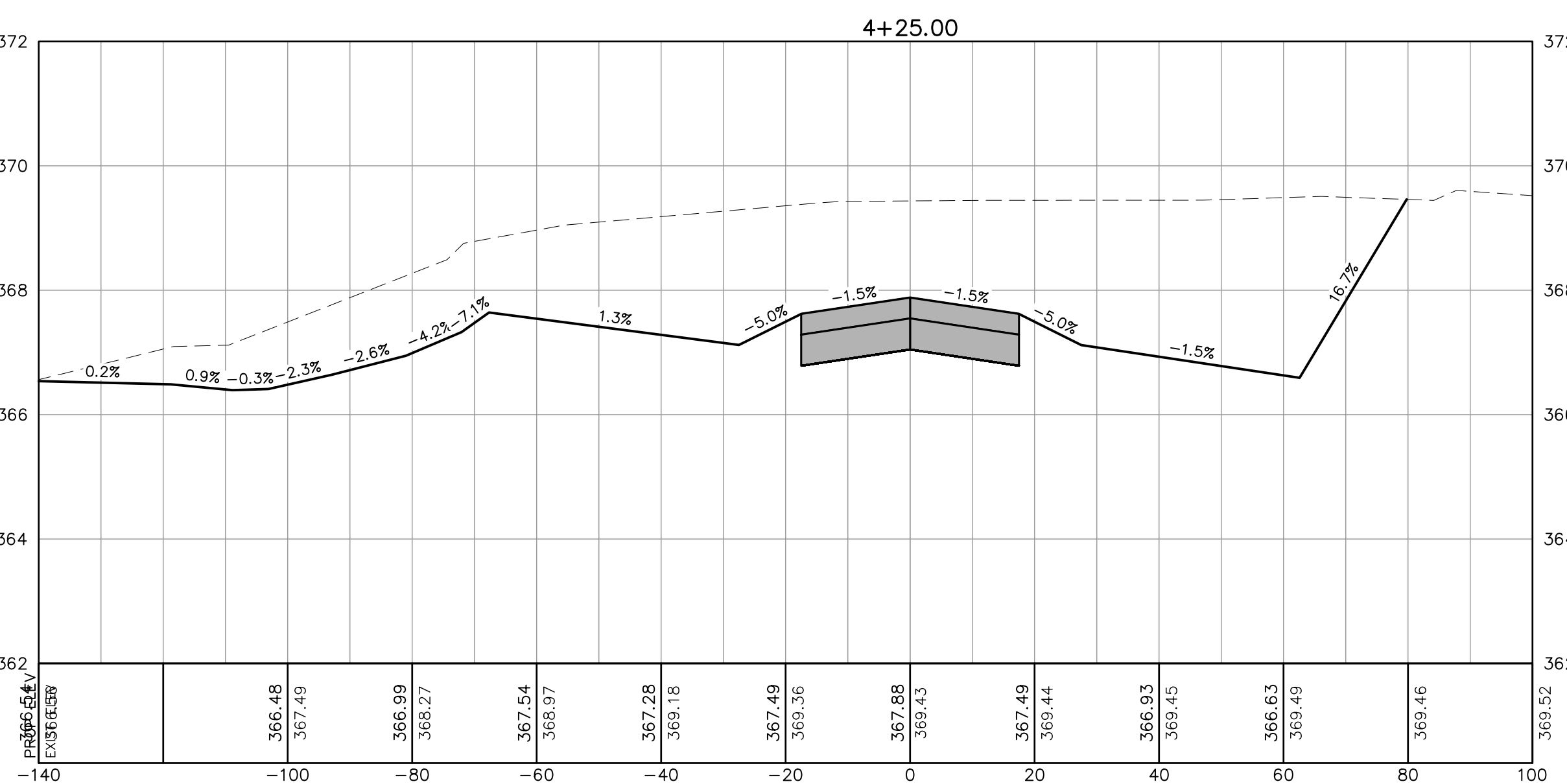
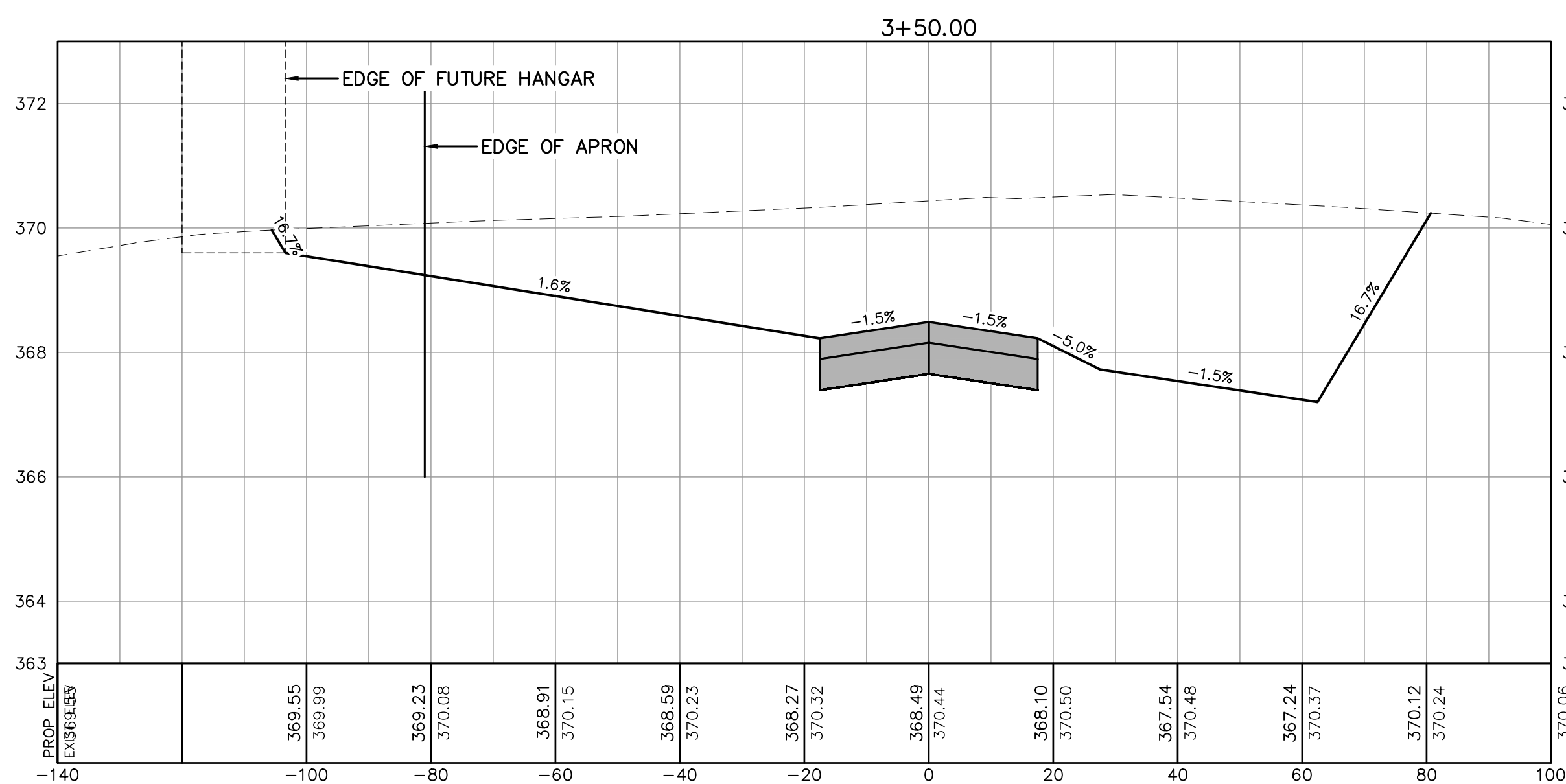
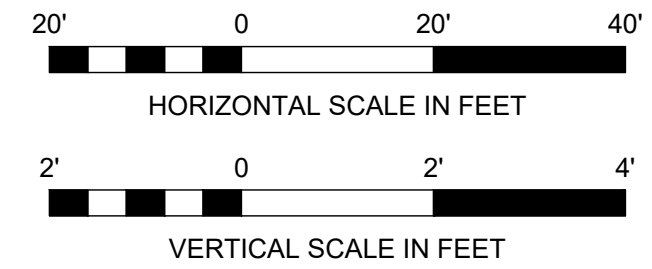
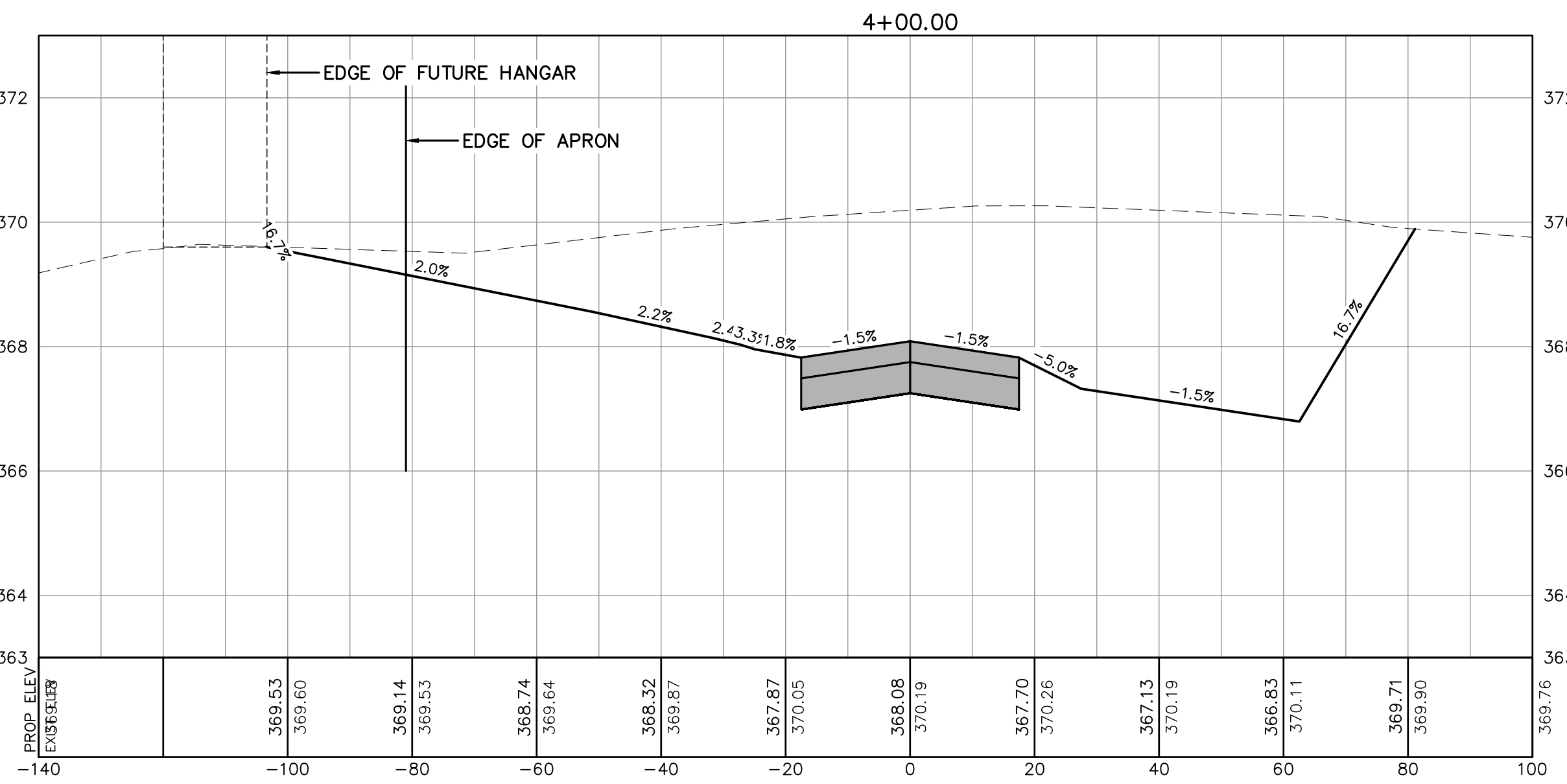
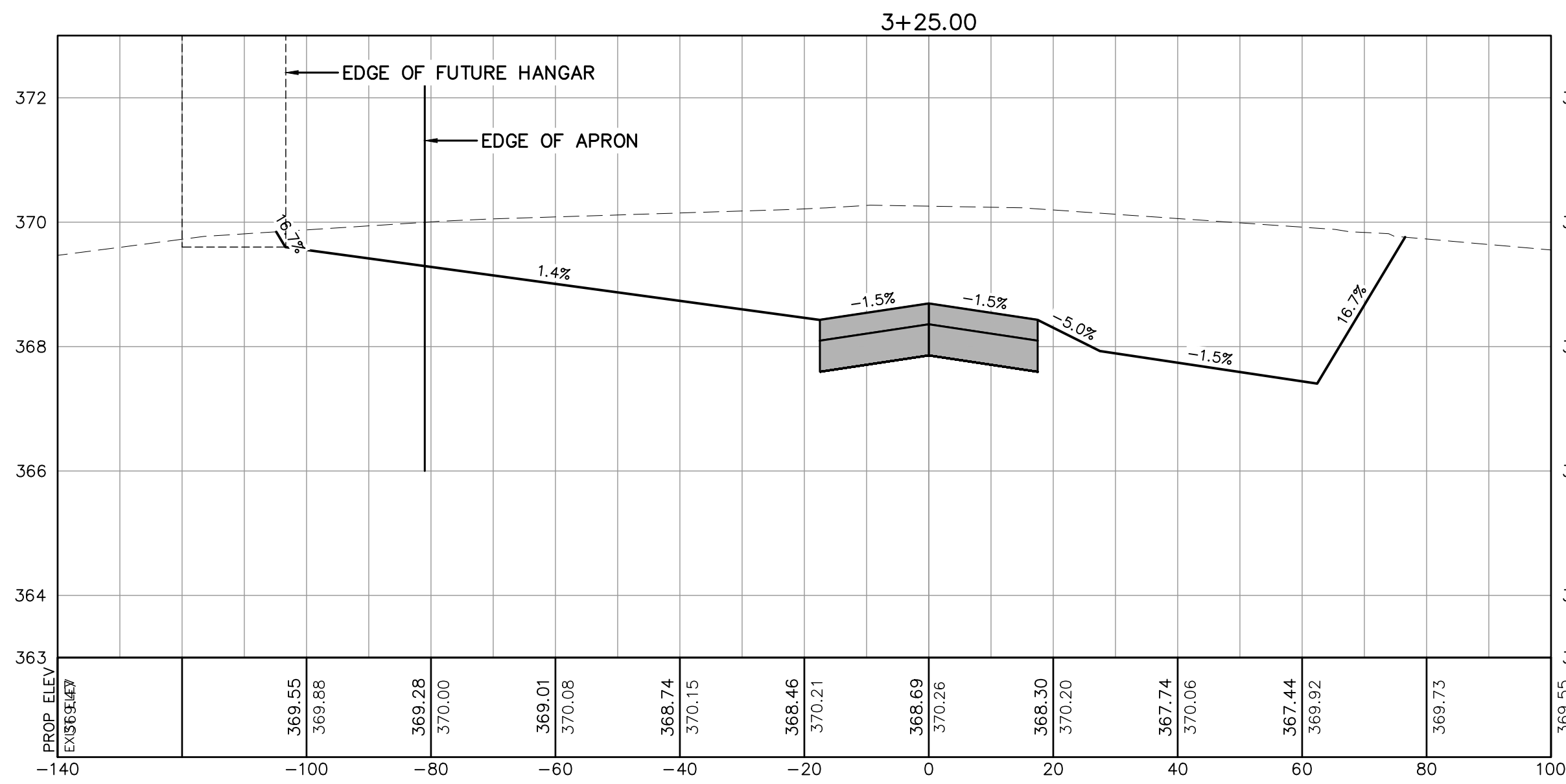
EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
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TAXIWAY CROSS SECTIONS II

EAST TEXAS REGIONAL AIRPORT
 SOUTHWEST GA AREA
 TAXILANE - PHASE I
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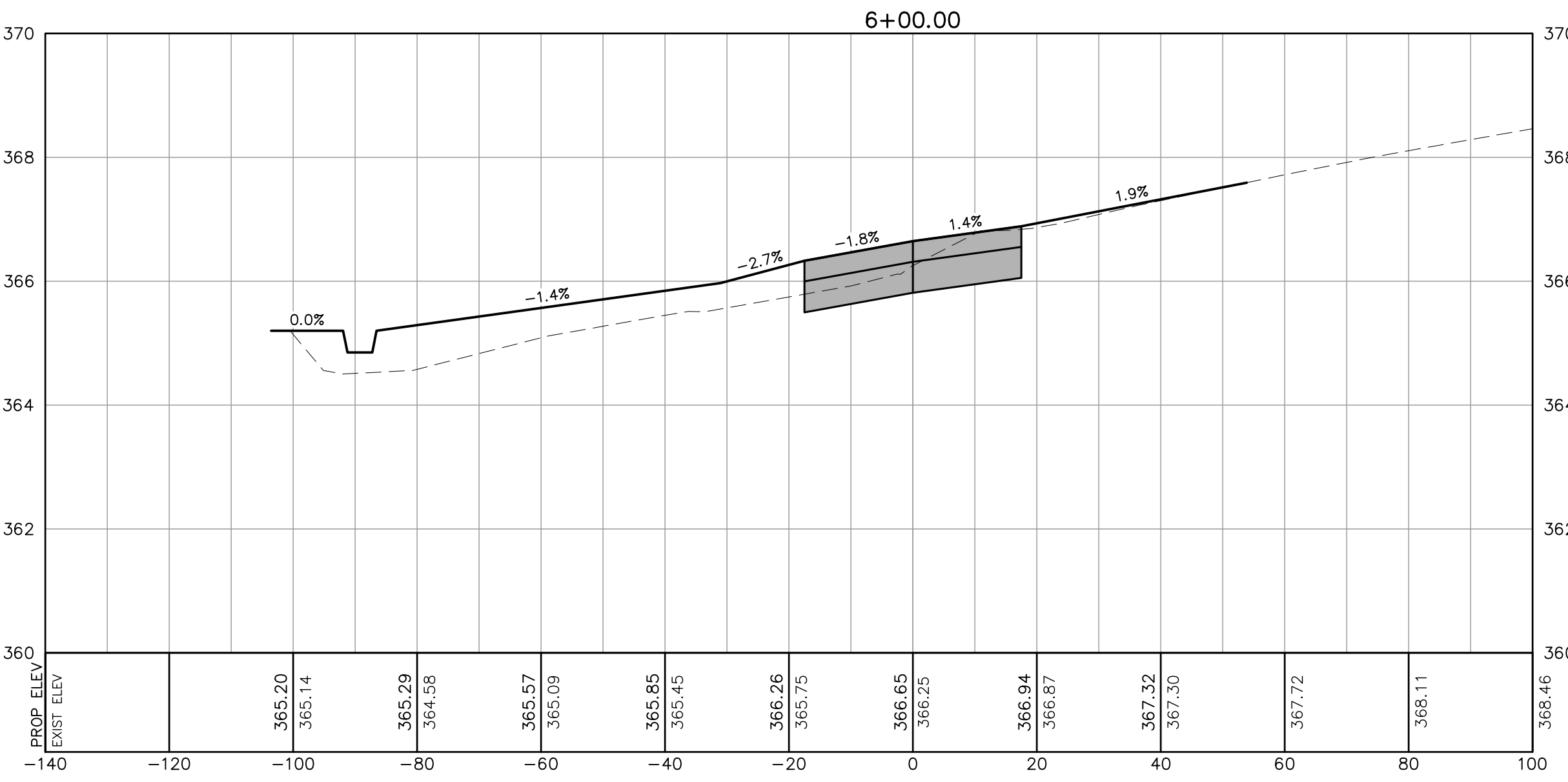
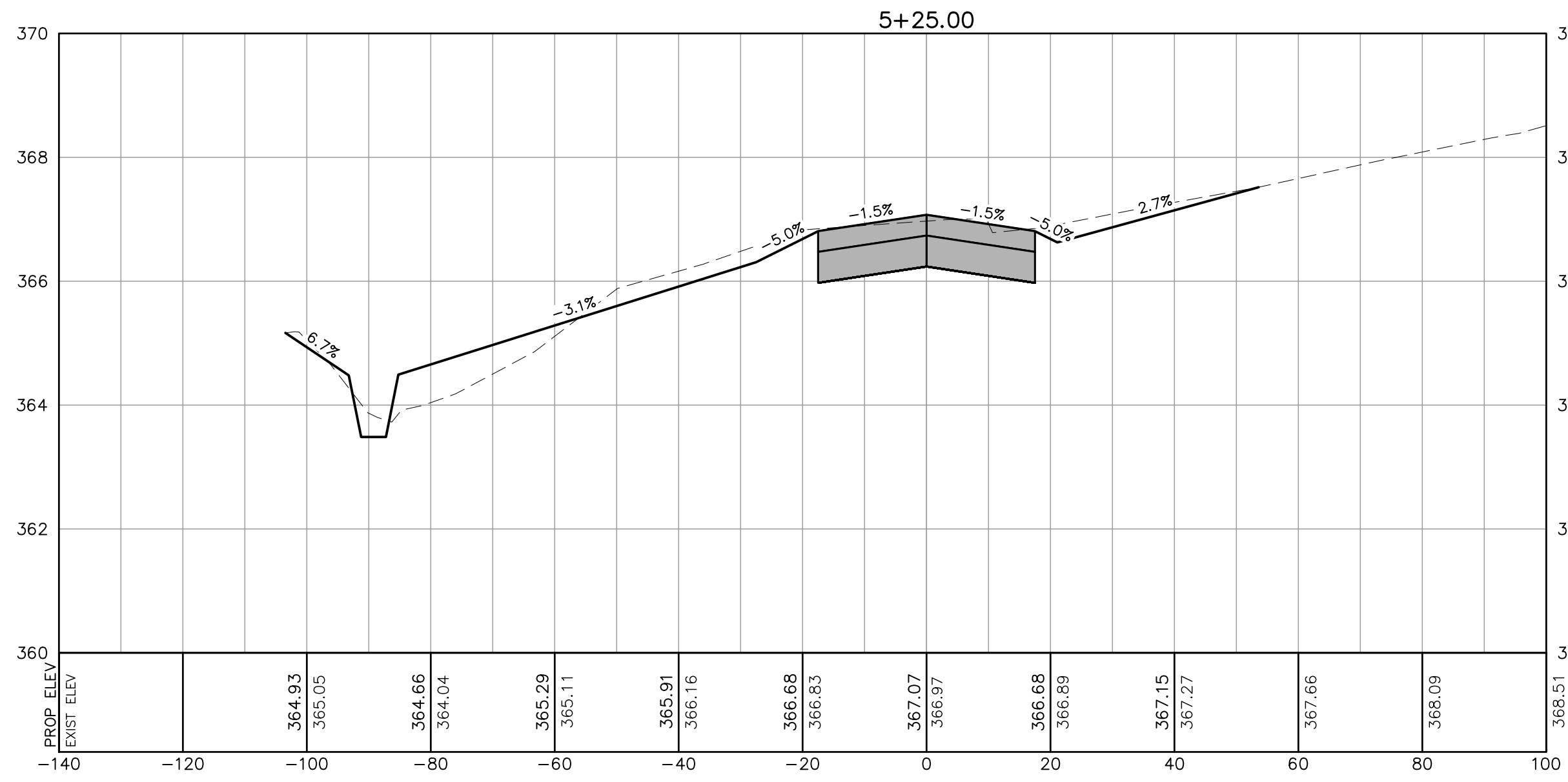
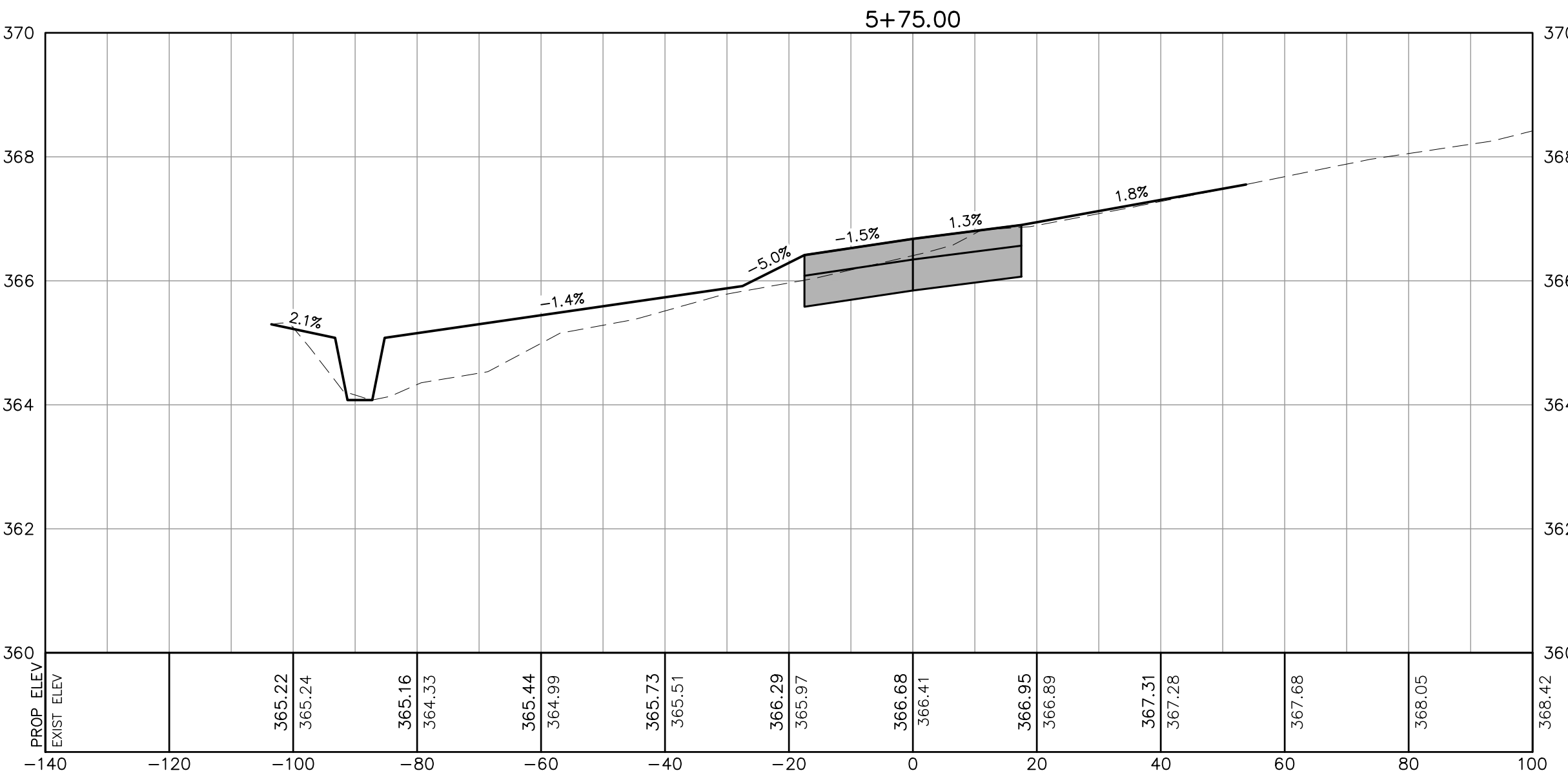
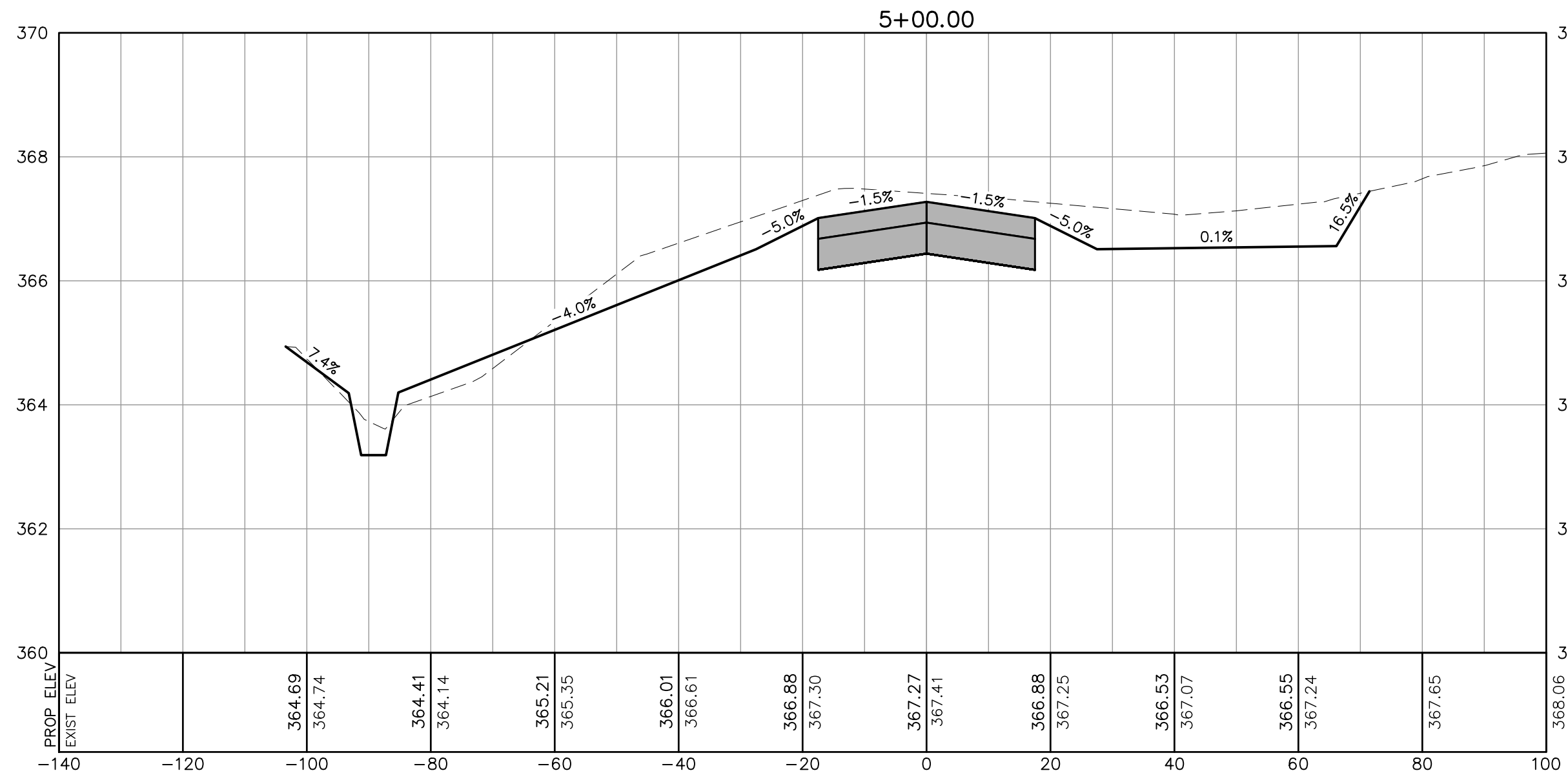
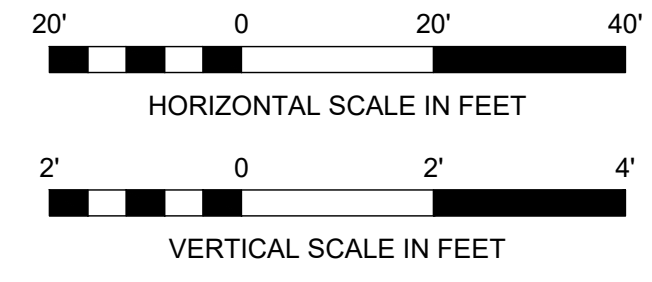
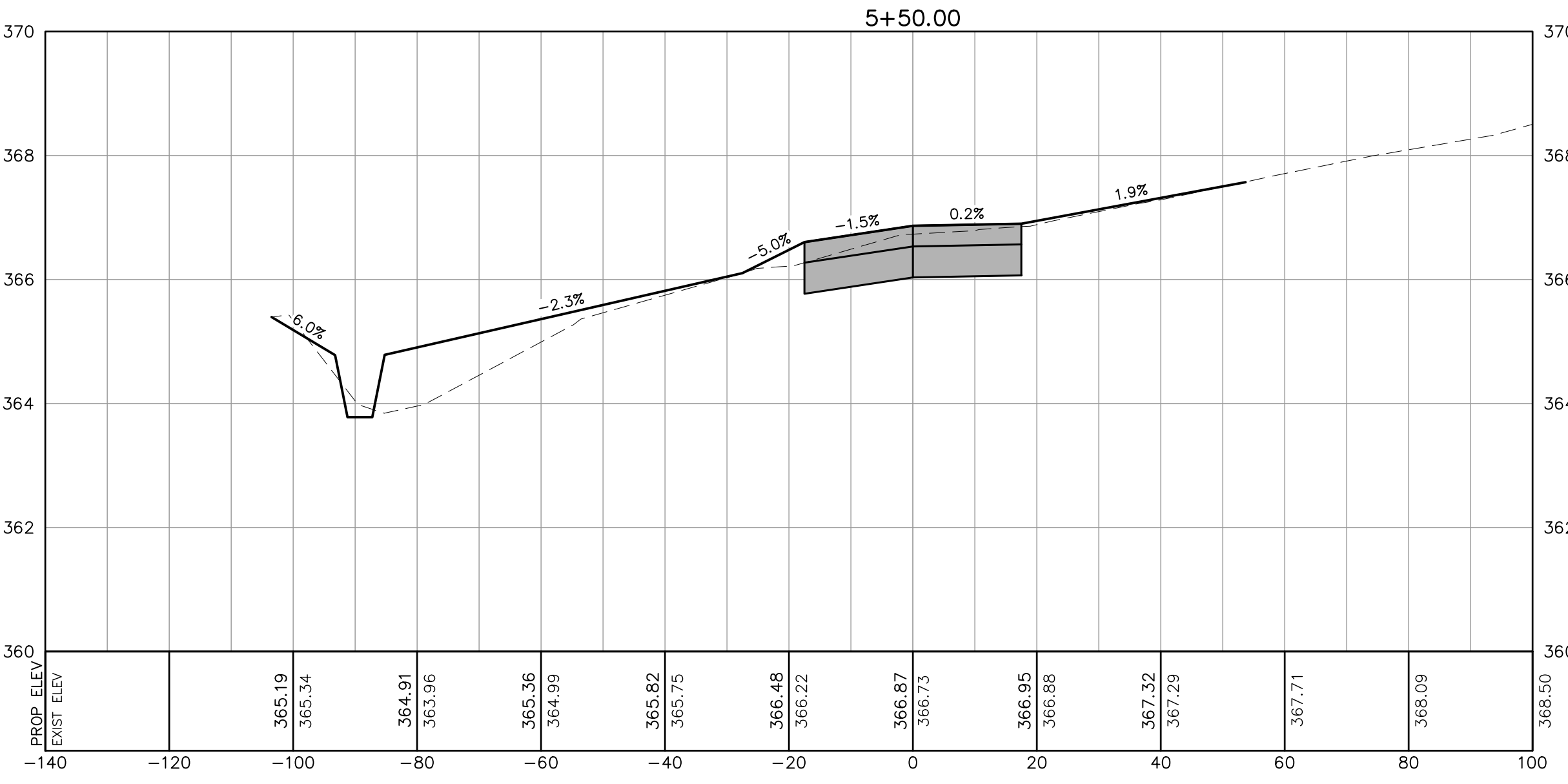
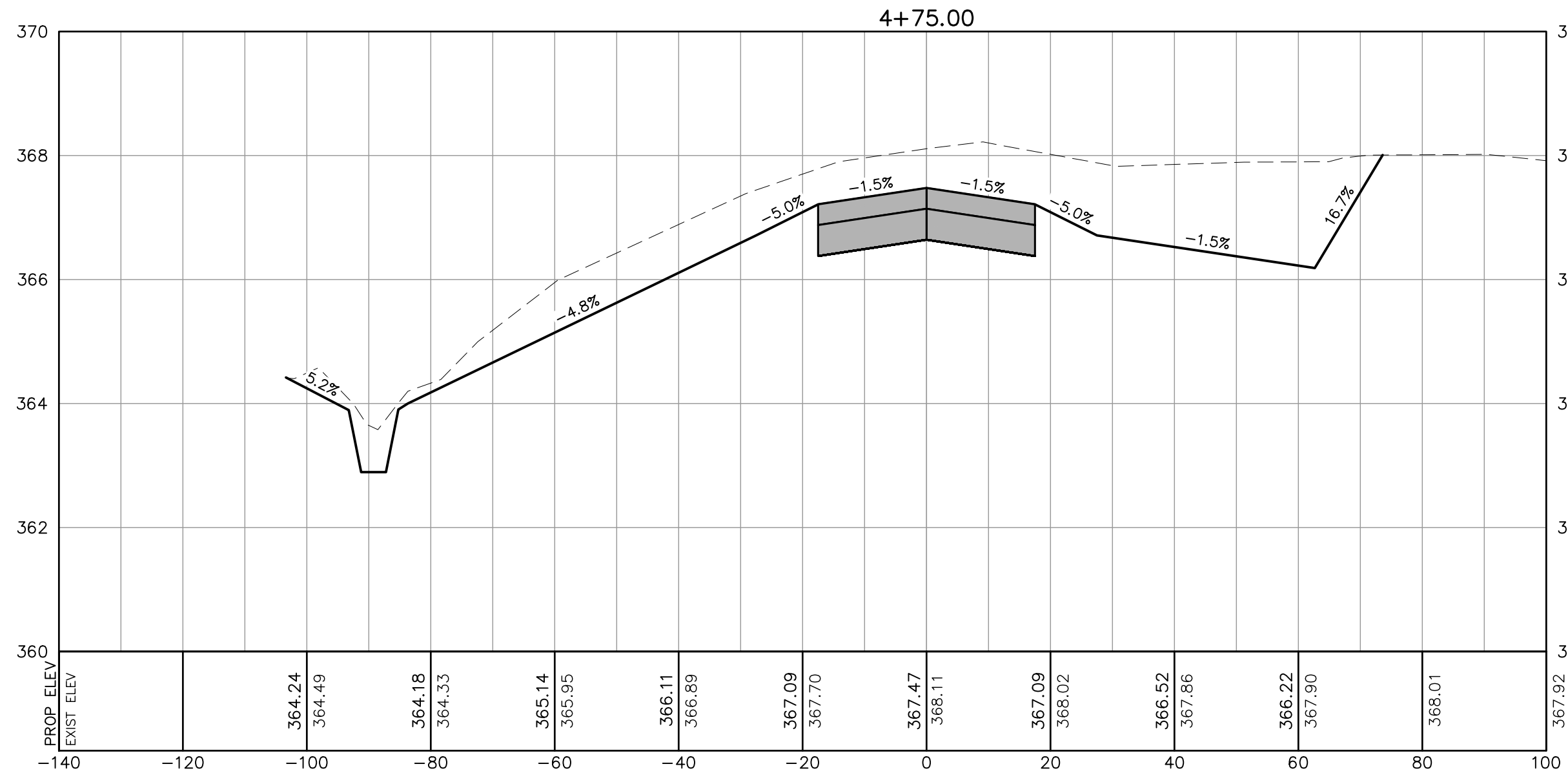
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STATE OF TEXAS
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SEAL: TBPE Firm Registration No. F-1356
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PROJECT TITLE: SOUTHWEST GA AREA TAXILANE - PHASE I - LONGVIEW, TEXAS
 SHEET NAME: GC-107

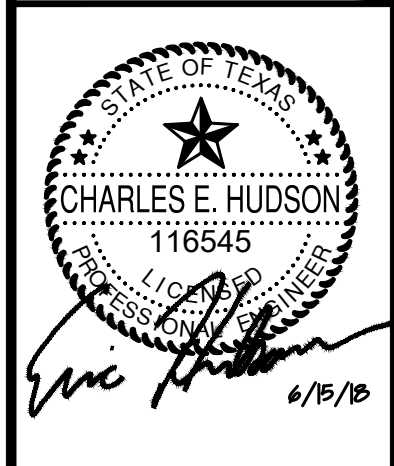


MARK	REVISION	DATE

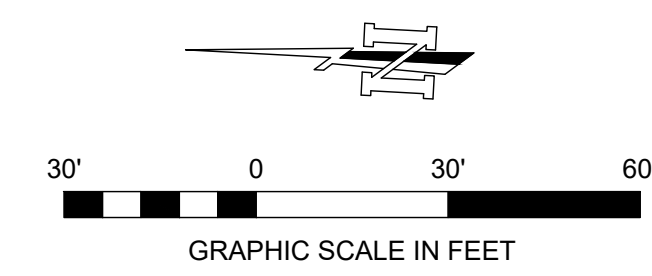
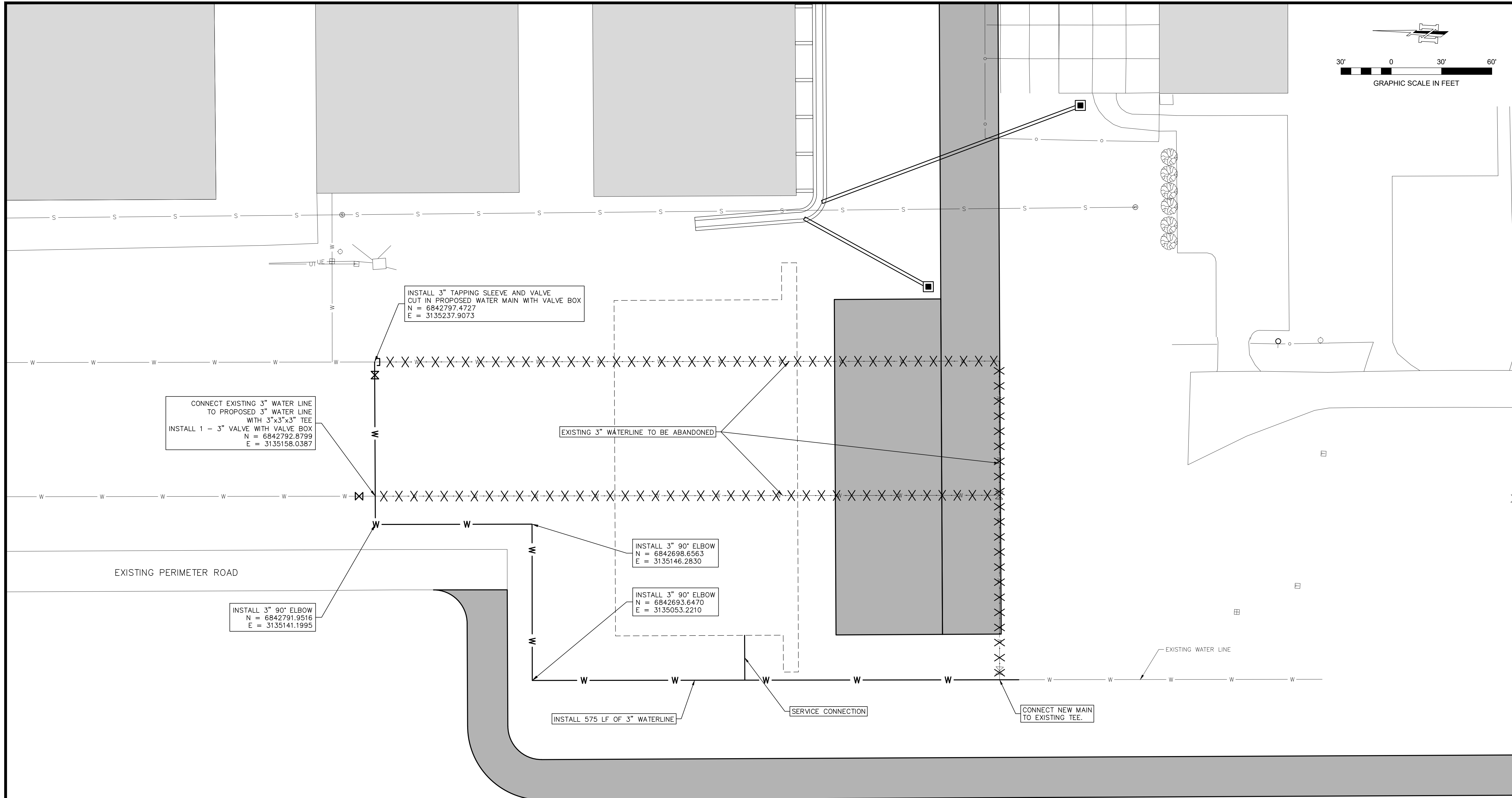
TAXIWAY CROSS SECTIONS III

EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
LONGVIEW, TEXAS

DRAWN BY: JKP	DESIGNED BY: CEH	LATEST REVISION: 6/8/2018	KSAs JOB NO.: GC.107
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TBPE Firm Registration No. F-1356
SHEET NO.
11



WATER LINE NOTES:

- UNLESS OTHERWISE DIRECTED, LINE SHALL BE INSTALLED WITH 36" MIN. COVER AND 72" MAX. COVER. PIPE SHALL BE AWWA C900 DR18 PVC PIPE AS SHOWN ON PLANS WITH RUBBER GASKET JOINTS. THE LINE SHALL BE INSTALLED AT THE LOCATION SHOWN ON THE PLANS. THE CONTRACTOR IS TO MAINTAIN SERVICE TO CUSTOMERS AT ALL TIMES. THE LINE SHALL BE INSTALLED WITH 12 GAUGE SOLID TRACER WIRE. TRACER WIRE SPLICES SHALL UTILIZE A DBY-6 DIRECT BURY SPLICE KIT AS MANUFACTURED BY 3M.
- INSTALLED LINE SHALL BE HYDROSTATICALLY TESTED AT 150 PSI FOR 4 HOURS. ALL APPARENT LEAKS SHALL BE REPAIRED. THE HYDROSTATIC LEAKAGE RATE FOR POLYVINYL CHLORIDE (PVC) PIPE AND APPURTENANCES SHALL NOT EXCEED THE AMOUNT ALLOWED BY AWWA C605. $Q=LD(P)^{1/2} / 148,000$ WHERE Q IS THE QUANTITY OF MAKEUP WATER IN GALLONS PER HOUR, L IS THE LENGTH OF PIPE SECTION BEING TESTED IN FEET, D IS THE NOMINAL DIAMETER OF THE PIPE IN INCHES, AND P IS THE AVERAGE TEST PRESSURE DURING THE HYDROSTATIC TEST IN POUNDS PER SQUARE INCH.
- LINE SHALL BE STERILIZED USING A MINIMUM 25 PPM CHLORINE SOLUTION FOR A PERIOD OF 24 HOURS. STERILIZED LINE SHALL BE FLUSHED TO REDUCE CHLORINE CONCENTRATION TO 0.2 PPM MAXIMUM, THEN SAMPLED FOR BACTERIOLOGICAL ANALYSIS. SAMPLE(S) SHALL BE TRANSPORTED TO TEXAS DEPARTMENT OF HEALTH LAB FOR ANALYSIS. RESULTS SHALL BE RETURNED TO ENGINEER. LINE SHALL BE FLUSHED, RESTERILIZED AND RETESTED AS NECESSARY TO OBTAIN TEXAS DEPARTMENT OF HEALTH APPROVAL. SEE SPECIFICATION SECTION 02675, ENTITLED "DISINFECTION OF WATER DISTRIBUTION SYSTEM."
- THE LOCATION OF EXISTING UTILITIES SHOWN ON THE PLANS IS APPROXIMATE. THE CONTRACTOR SHALL VERIFY THE LOCATION AND PRESENCE OF ALL UTILITIES. UTILITIES NOT SHOWN ON THE PLANS MAY BE PRESENT. THE CONTRACTOR SHALL ANTICIPATE THE PRESENCE OF EXISTING UTILITIES IN THE LOCATION OF THE PROPOSED WATER LINE. NOTIFY TELEPHONE COMPANIES, PIPELINE COMPANIES, AND RAILROAD COMPANIES A MINIMUM OF 48 HOURS BEFORE CONSTRUCTION IN THE VICINITY OF THEIR LINES OR RIGHT-OF-WAYS.
- GATE VALVES SHALL BE RESILIENT SEAT OR RESILIENT WEDGE MECHANICAL JOINT GATE VALVES WITH 2" SQUARE NUT OPERATORS IN ACCORDANCE WITH AWWA C509 OR C515.
- ALL TRENCHES SHALL BE BACK FILLED IMMEDIATELY AFTER PLACEMENT OF THE PIPE. EXCAVATED SOIL SHALL NOT BE LEFT ON PUBLIC R-O-W OVER NIGHT.
- EXISTING WATER LINES SHALL REMAIN IN SERVICE UNTIL PROPOSED LINE IS TESTED AND APPROVED FOR SERVICE BY THE ENGINEER. AT THAT TIME SERVICES MAY BE TRANSFERRED TO THE PROPOSED LINE. SERVICE SHALL BE INTERRUPTED ONLY AS NECESSARY FOR TRANSFER TO THE PROPOSED LINE.

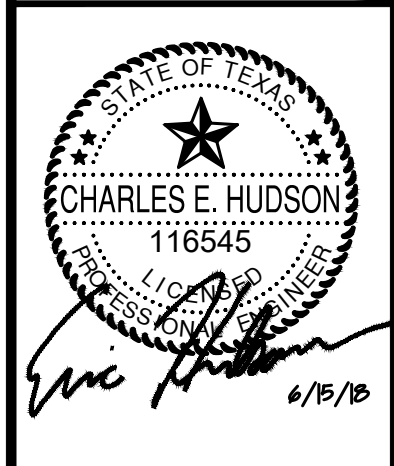
LEGEND	
	PROPOSED WATERLINE
	EXISTING WATERLINE
	EXISTING BUILDING

MARK	REVISION	DATE

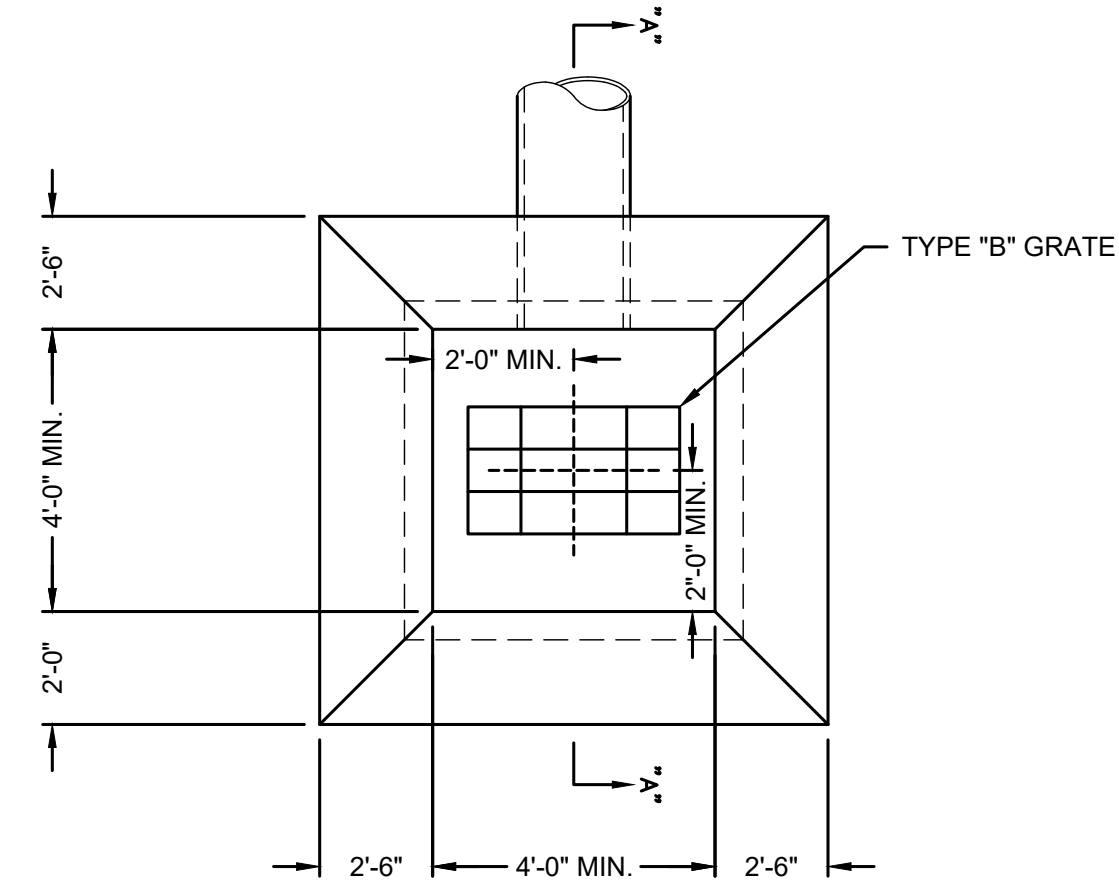
UTILITY PLAN

**EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
LONGVIEW, TEXAS**

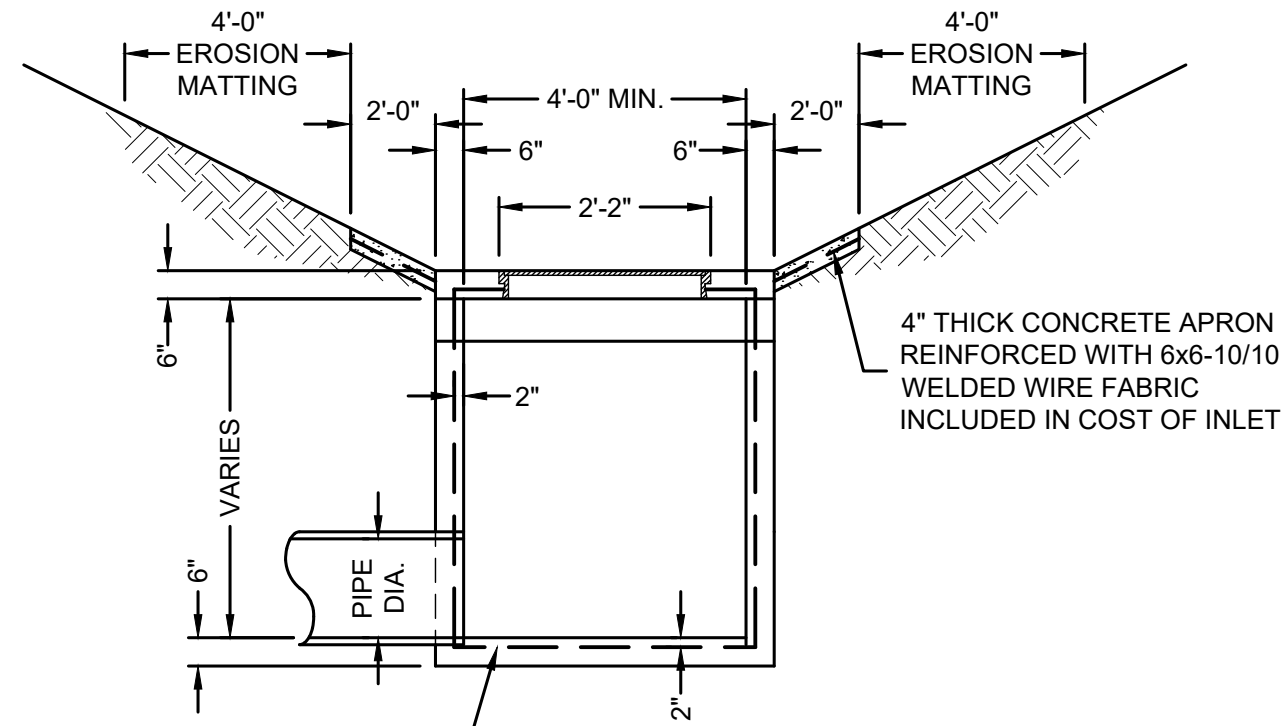
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PLAN VIEW



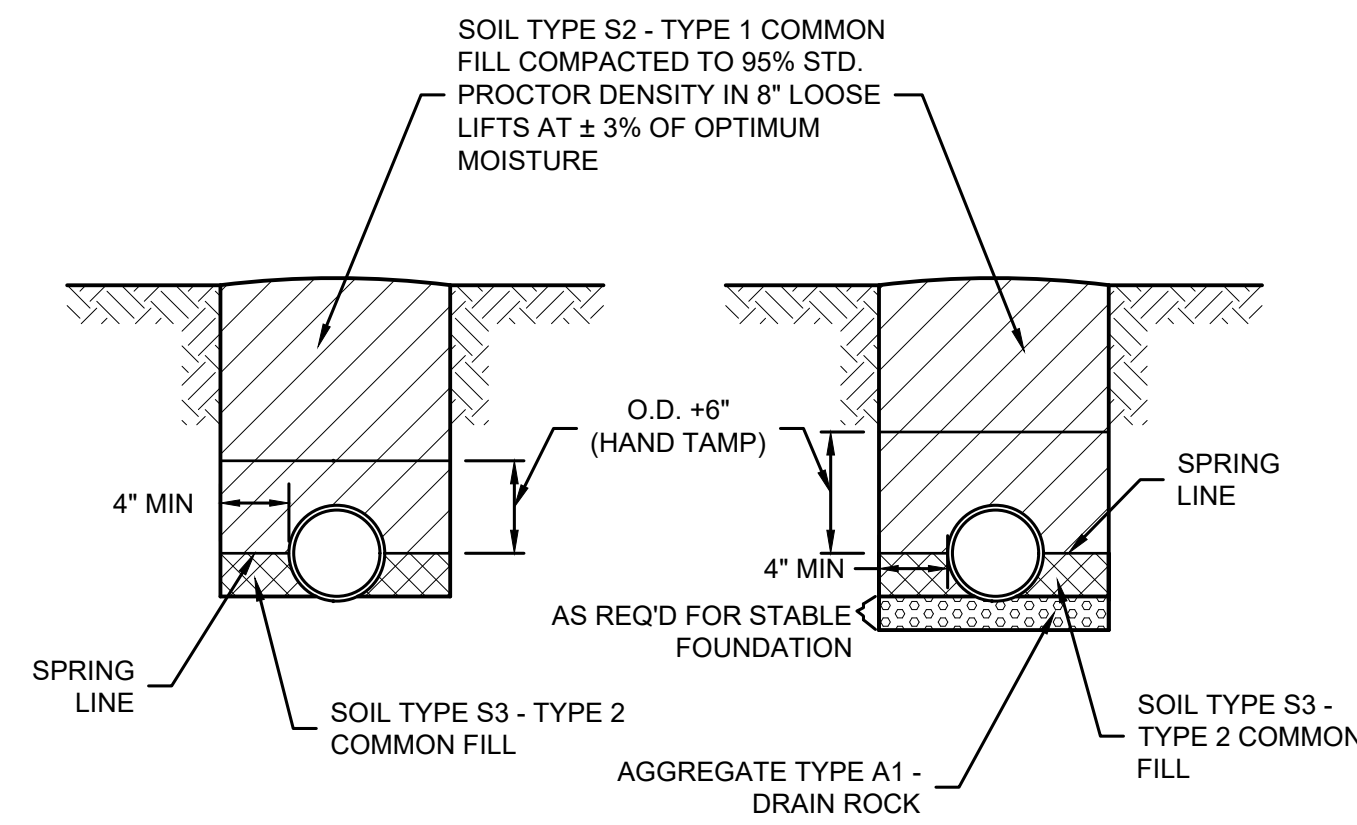
SECTION "A-A"

BOX TO BE REINFORCED WITH #4 BARS @ 8" O.C. EACH WAY

NOTES:

1. ALL CORNERS TO CHAMFERED 3/4".
2. RUBBED FINISH REQUIRED ON EXPOSED SURFACES.
3. R.C. CATCH BASIN SHALL BE STRUCTURAL CONCRETE AND HAVE A COMPRESSIVE STRENGTH OF 3500 P.S.I. IN 28 DAYS.
4. PROVIDE SLAB TYPE FRAME AND GRATE (MIN. 350 LBS. TOTAL WEIGHT).
5. CAST IRON FRAME AND GRATE TO BE BITUMINOUS COATED.
6. REINFORCING BARS SHALL CONFORM TO A.S.T.M. SPECIFICATIONS.
7. GRATES TO BE CENTERED.
8. BACKFILL TO BE COMPACTED TO 95% SPECIFIED DENSITY AT OPTIMUM MOISTURE.

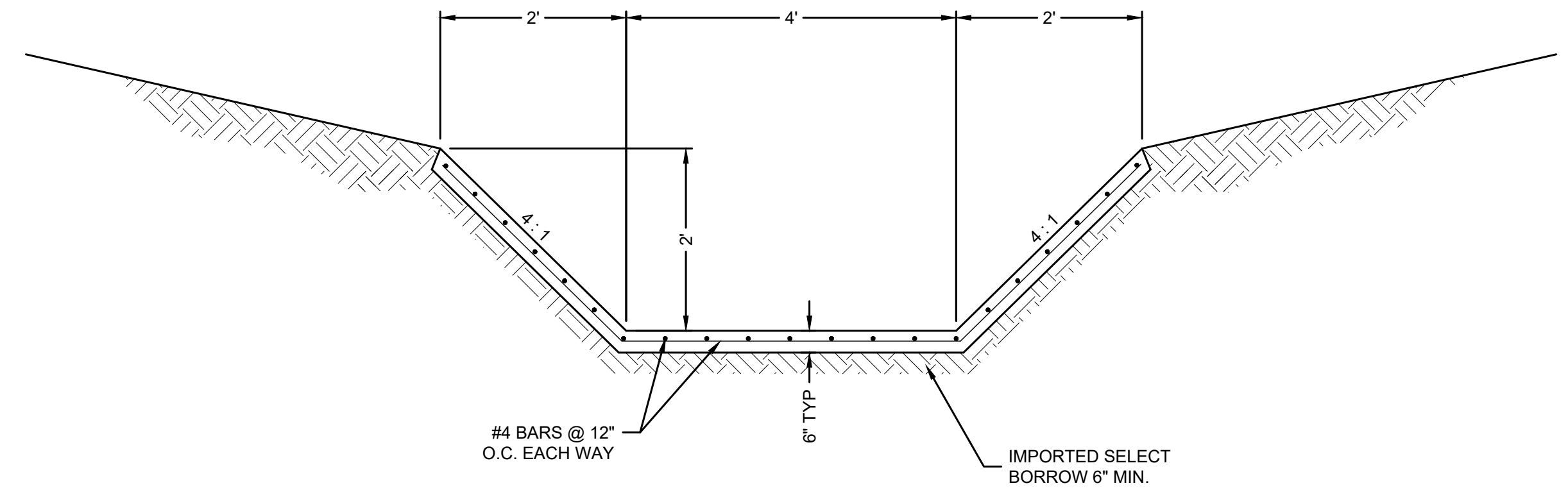
1 INLET DETAIL
N.T.S.



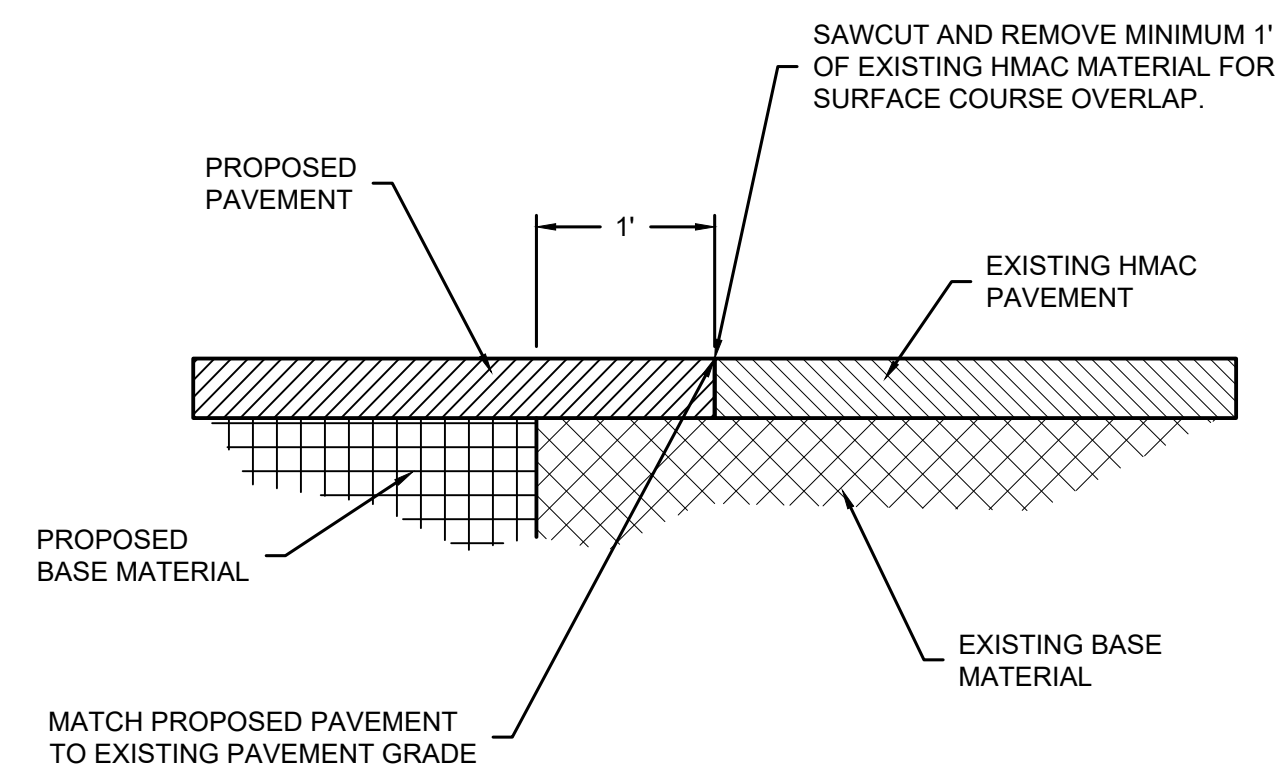
CLASS D BEDDING
(USE UNDER NORMAL CONDITIONS)

CLASS D BEDDING WITH FOUNDATION
(USE WHEN DIRECTED BY ENGINEER)

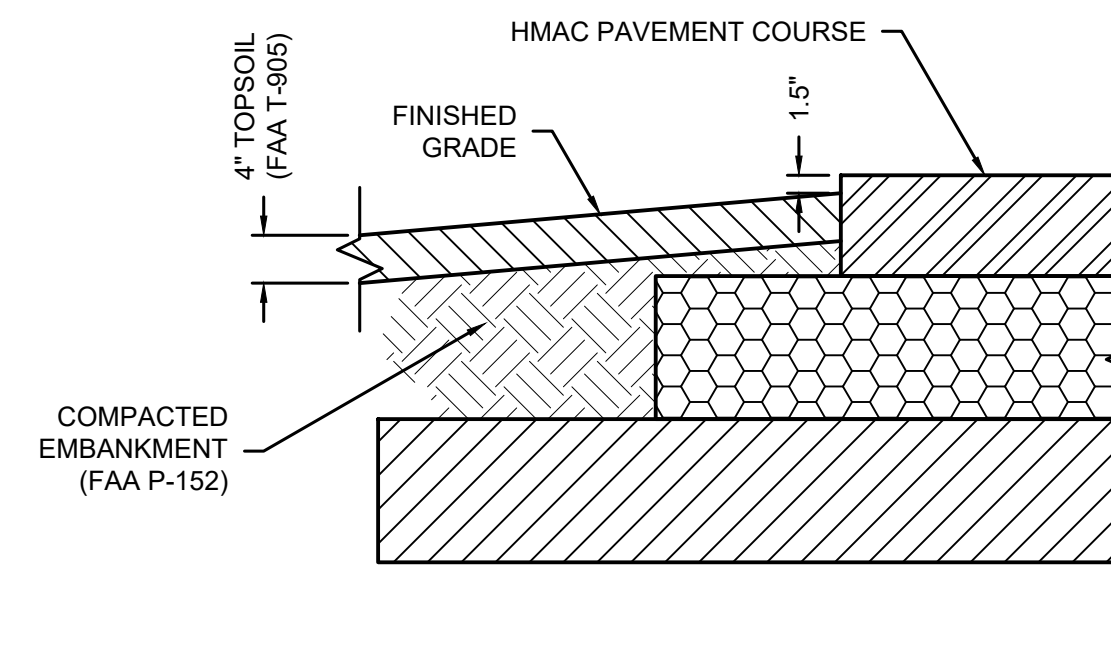
2 EMBEDMENT DETAILS FOR STORM DRAIN LINES
N.T.S.



3 CONCRETE FLUME
N.T.S.



4 SAWCUT DETAIL
N.T.S.



PAVEMENT DROP-OFF NOTES:

1. PROVIDE 1.5" DROP-OFF FROM TOP OF PAVEMENT TO ADJACENT FINISHED GRADE AT EDGE OF ALL PAVEMENT CONSTRUCTED IN THIS PROJECT
2. SUFFICIENT TOPSOIL MAY NOT BE AVAILABLE ON THE AIRPORT SITE. THE CONTRACTOR SHALL SUPPLY TOPSOIL FROM OFF-SITE AS REQUIRED. TOPSOIL MUST MEET THE REQUIREMENTS OF FAA T-905
3. THE TOPSOIL SHALL BE LIGHTLY ROLLED INTO PLACE
4. ALL TOPSOILED AREAS SHALL BE TREATED WITH SEED AND FERTILIZER IN ACCORDANCE WITH FAA T-905
5. UPON COMPLETION OF CONSTRUCTION, INSTALL 4' OF EROSION CONTROL MATTING (P-156) ADJACENT TO ALL PROPOSED PAVEMENT EDGES.

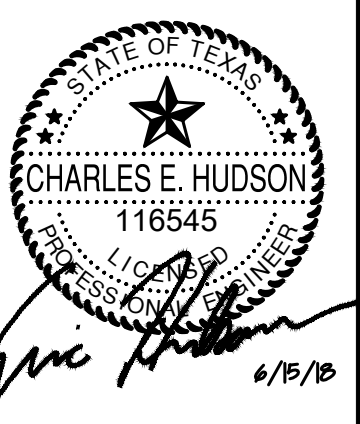
5 PAVEMENT DROP-OFF DETAIL
N.T.S.

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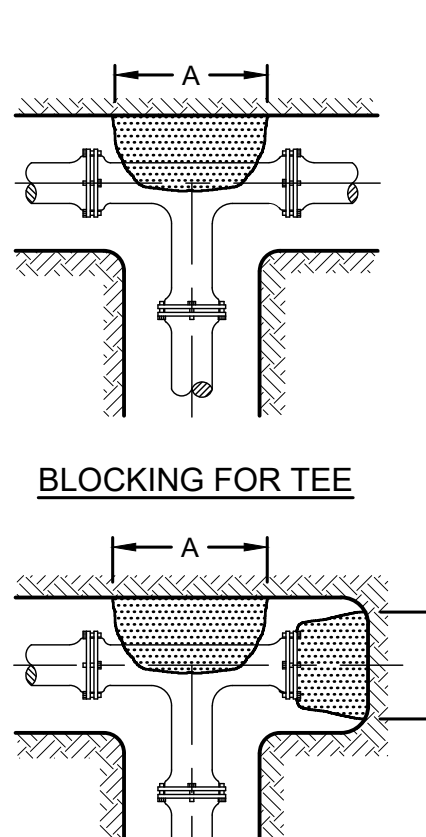
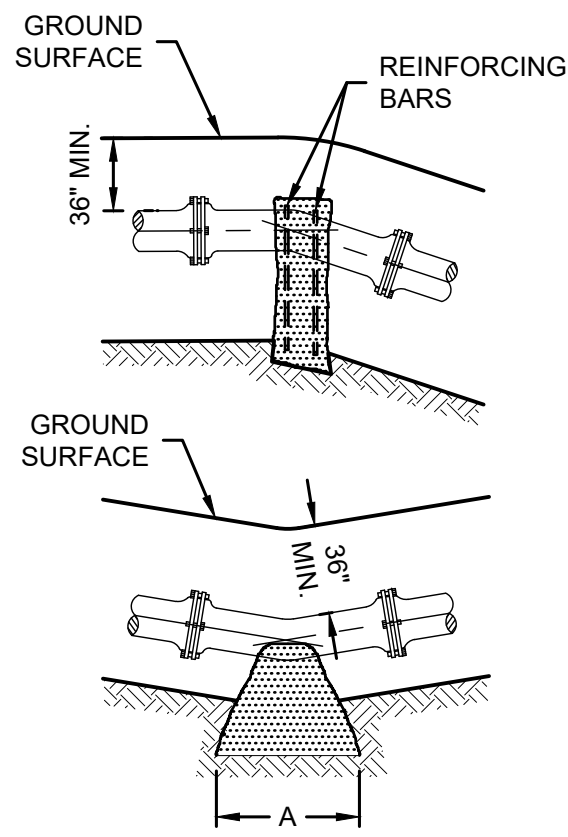
PROJECT TITLE: SOUTHWEST GA AREA TAXILANE - PHASE I - LONGVIEW, TEXAS

EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
LONGVIEW, TEXAS

DRAWN BY:	JKP
DESIGNED BY:	CEH
LATEST REVISION:	6/12/2018
ASA JOB NO.:	GC.107



SEAL: TBPE Firm Registration No. F-1356
SHEET NO.



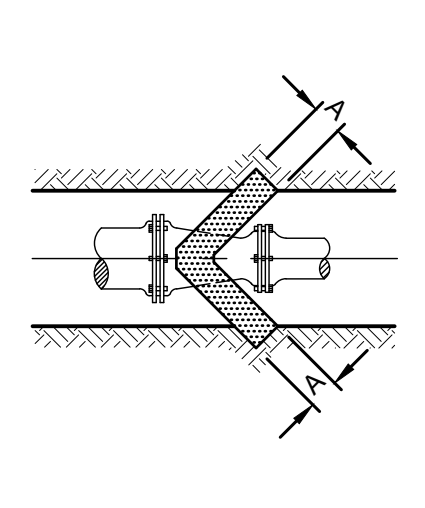
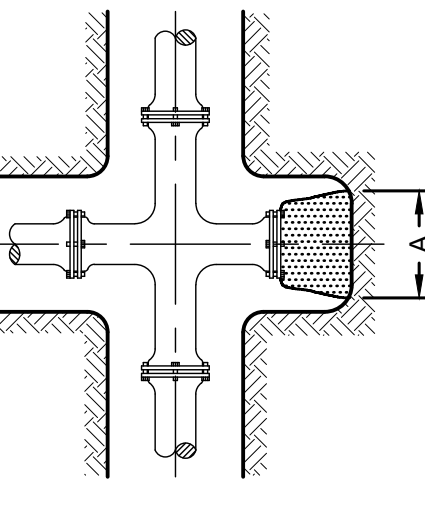
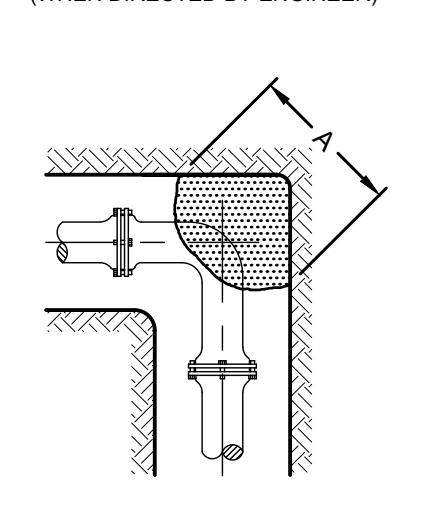
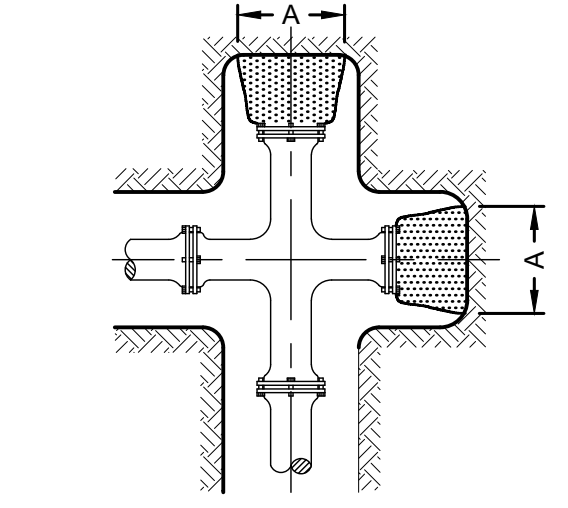
- NOTES ON THRUST BLOCKING**
- ALL BLOCKING SHALL BE AGAINST UNDISTURBED HAND DUG SOIL AND SHALL BE CONCRETE HAVING A MINIMUM 28 DAY STRENGTH OF 3000 LB. PER SQUARE INCH.
 - THRUST CALCULATIONS TO BE BASED ON THRUST DUE TO WATER PRESSURE AT 100% OF TEST PRESSURE. $THRUST = 2 AP \sin 1/2 \theta$, WHERE A = AREA OF PIPE, P = WATER PRESSURE, θ = DEFLECTION ANGLE.
 - VERTICAL UPLIFT BLOCKS SHALL BE DESIGNED ON THE BASIS OF 150 LBS. PER CU. FT. FOR CONCRETE AND SOIL AT 120 LBS. PER CU. FT. OVER THE AREA OF BLOCK.
 - VERTICAL DOWN THRUST BLOCKS SHALL BE DESIGNED ON THE BASIS OF 3000 LB. PER SQ. FT. ALLOWABLE SOIL BEARING PRESSURE. DIMENSIONS MAY BE DECREASED WITH APPROVAL OF THE ENGINEER OR MEASURED SOIL CONDITIONS PERMIT. IN POOR SOIL CONDITIONS, BLOCK DIMENSIONS SHALL BE INCREASED IN PROPORTION TO ALLOWABLE BEARING VALUE.
 - THRUST BLOCKS ON HORIZONTAL BENDS, TEES, CROSSES, AND REDUCERS SHALL BE SIZED BASED ON 2400 LBS. PER SQ. FT. OF BLOCKING SURFACE AREA IN CONTACT WITH UNDISTURBED SOIL. BLOCK DIMENSIONS MAY BE DECREASED WITH APPROVAL OF THE ENGINEER OR MEASURED SOIL CONDITIONS PERMIT. IN POOR SOIL CONDITIONS, BLOCK DIMENSIONS SHALL BE INCREASED IN PROPORTION TO THE ALLOWABLE BEARING VALUE.
 - ALL BLOCKING SHALL HAVE A MINIMUM SOIL COVER OF 1 FT.
 - ADDITIONAL REINFORCING MAY BE REQUIRED FOR HORIZONTAL BLOCKING TO HANDLE UNUSUAL SHEAR LOADING CONDITIONS.

PIPE SIZE	TEES		BENDS						VERTICAL		REDUCERS	
	A	B*	90°		45°		22 1/2°		A	B*	A	B*
3"	30"	18"	60"	10"	24"	24"	18"	18"	24"	18"	24"	18"

* B = VERTICAL OR DEPTH DIMENSION PERPENDICULAR TO DIMENSION "A"

TYPICAL BLOCKING FOR VERTICAL BENDS

SPECIAL BLOCKING FOR TEE WITH PLUG (WHEN DIRECTED BY ENGINEER)



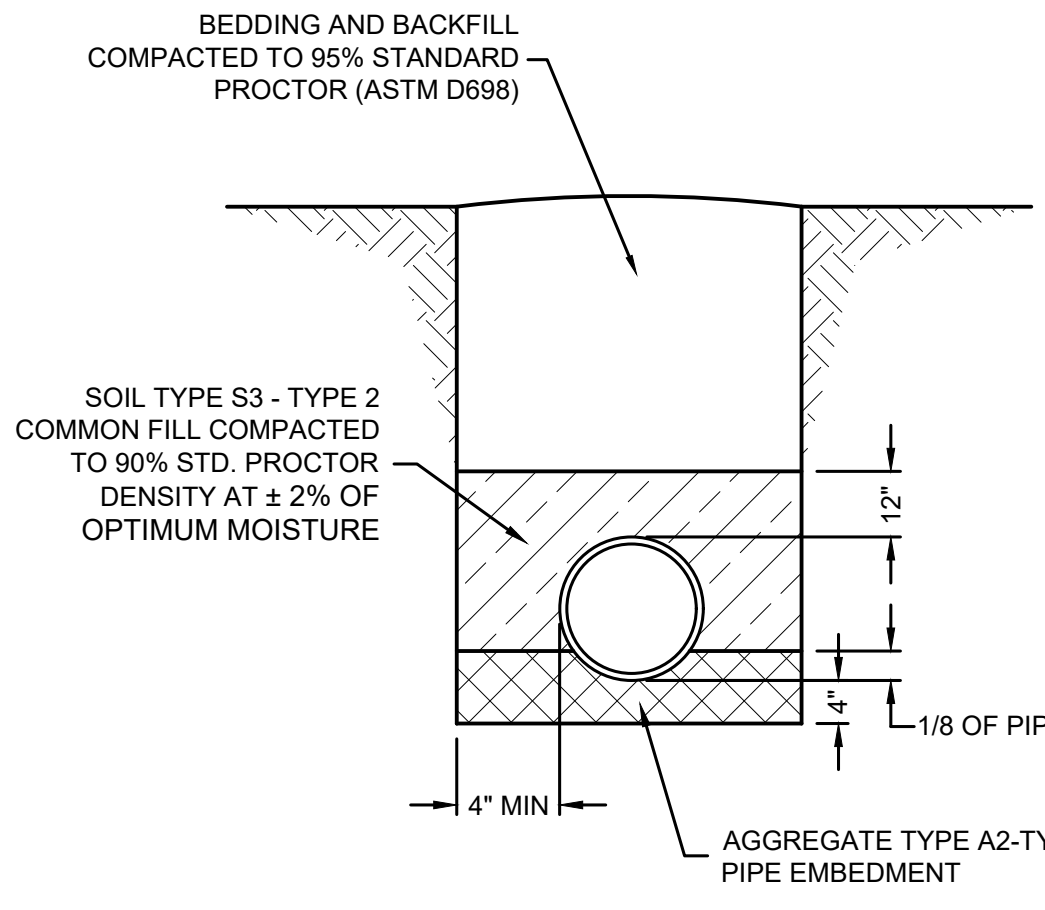
TYPICAL SECTION OF CROSS & 2 PLUG BLOCKING (WHEN DIRECTED BY ENGINEER)

TYPICAL BLOCKING FOR HORIZONTAL BENDS (WHEN DIRECTED BY ENGINEER)

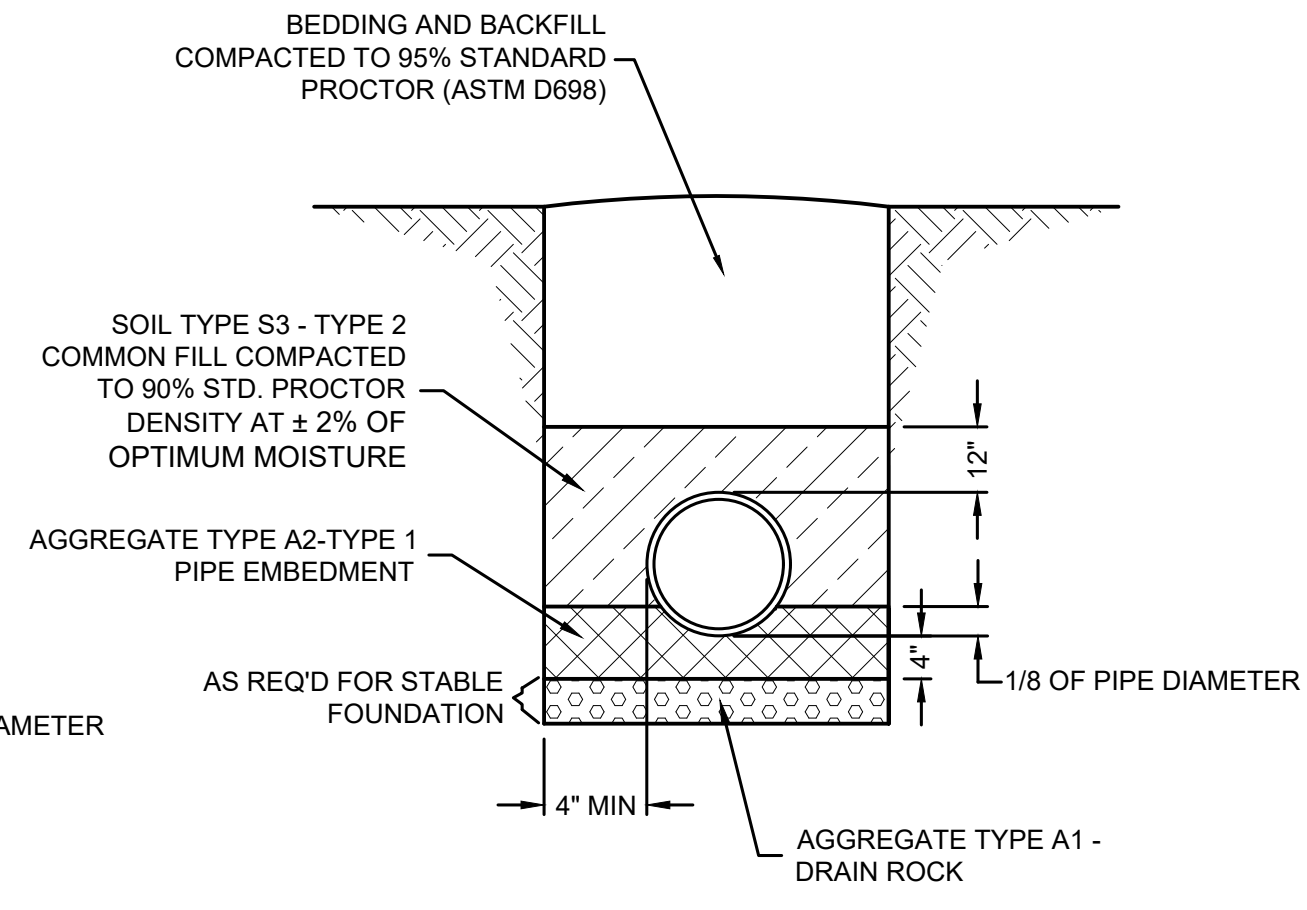
TYPICAL SECTION OF CROSS & BLOCKING (WHEN DIRECTED BY ENGINEER)

THRUST SUPPORT FOR REDUCER CONNECTION (SIZE TO BE DETERMINED BY ENGINEER)

1 THRUST BLOCKING
N.T.S.

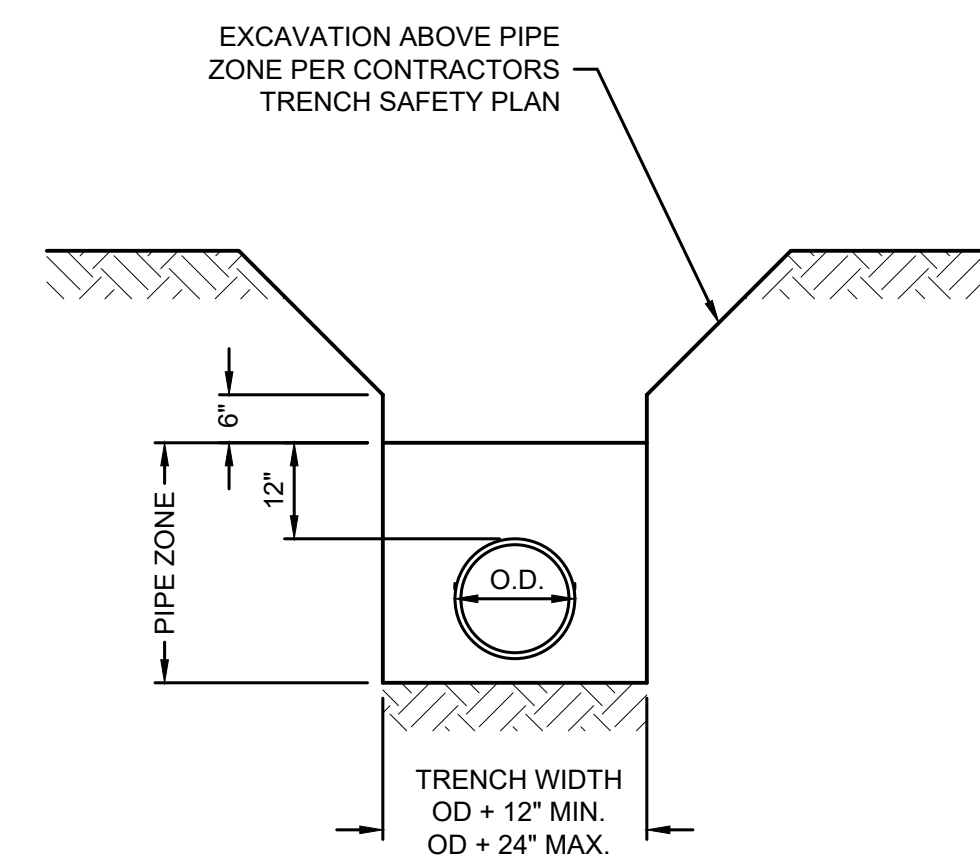


CLASS E BEDDING
(USE UNDER NORMAL CONDITIONS)



CLASS D BEDDING
(USE UNDER EXISTING ROAD AND DRIVEWAY SECTIONS AND WHEN DIRECTED BY ENGINEER)

2 EMBEDMENT DETAILS
N.T.S.



3 TYPICAL TRENCH SECTION
N.T.S.

MARK	REVISION	DATE

PROJECT TITLE: SOUTHWEST GA AREA TAXILANE - PHASE I LONGVIEW, TEXAS
SHEET NAME: UTILITY DETAILS

EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
LONGVIEW, TEXAS

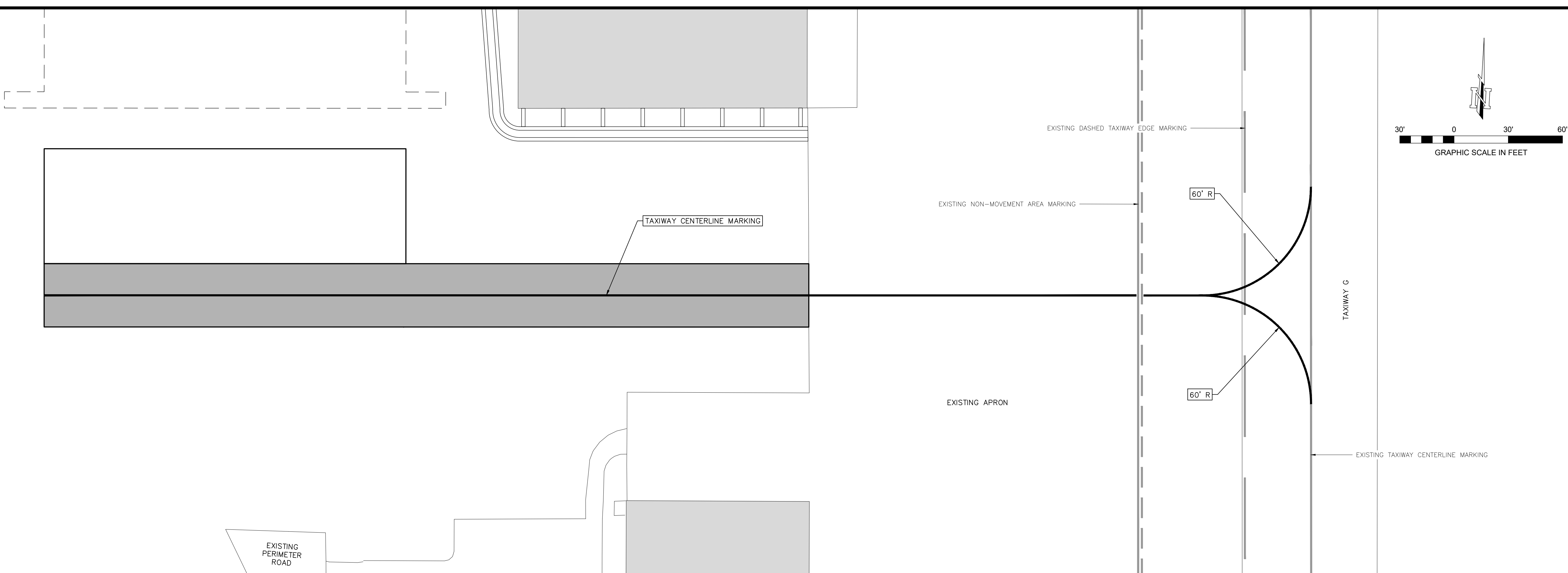
DRAWN BY: JKP	DESIGNED BY: CEH	LATEST REVISION: 6/13/2018	ASA JOB NO.: GC-107
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CHARLES E. HUDSON
116545
LICENSED PROFESSIONAL ENGINEER
6/15/18

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MARKING PLAN AND DETAILS

**EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
LONGVIEW, TEXAS**

DRAWN BY:	JKP
DESIGNED BY:	CEH
LATEST REVISION:	6/11/2018
ASA JOB NO.:	GC.107

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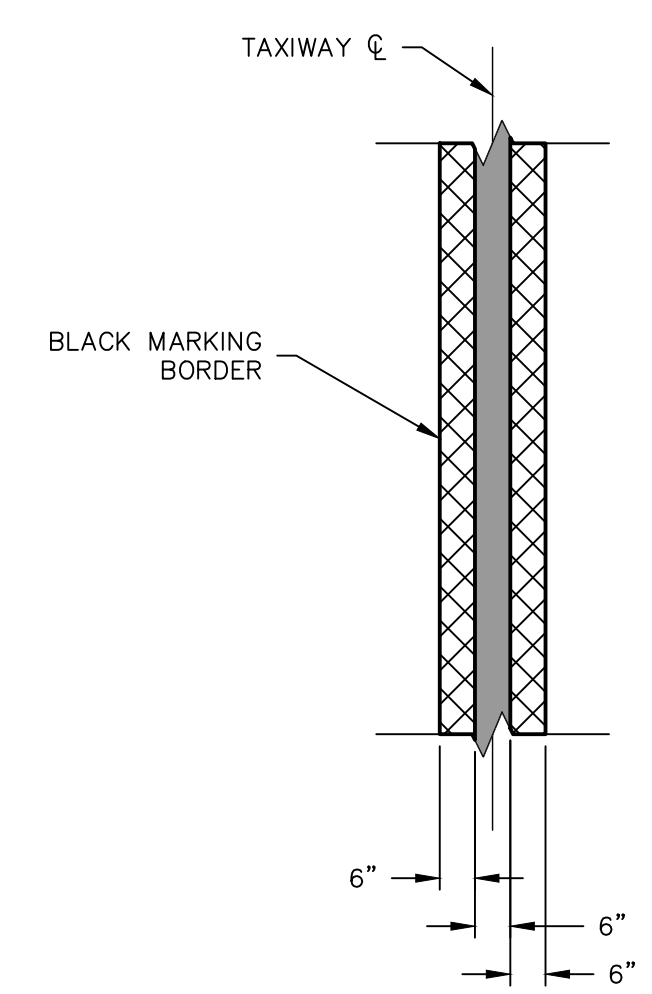
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PROJECT TITLE: SOUTHWEST GA AREA TAXILANE - PHASE I - LONGVIEW, TEXAS

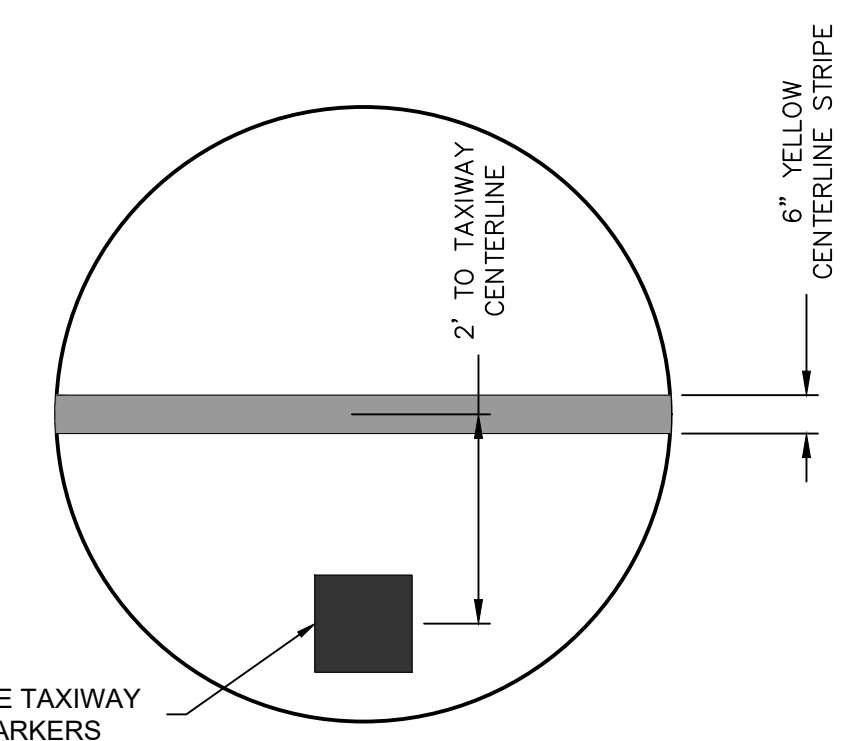
SHEET NAME:

PROJECT NAME:

GC.107



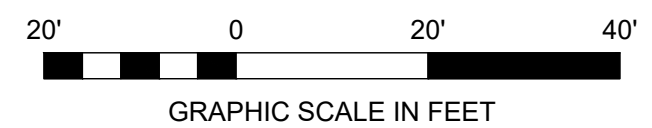
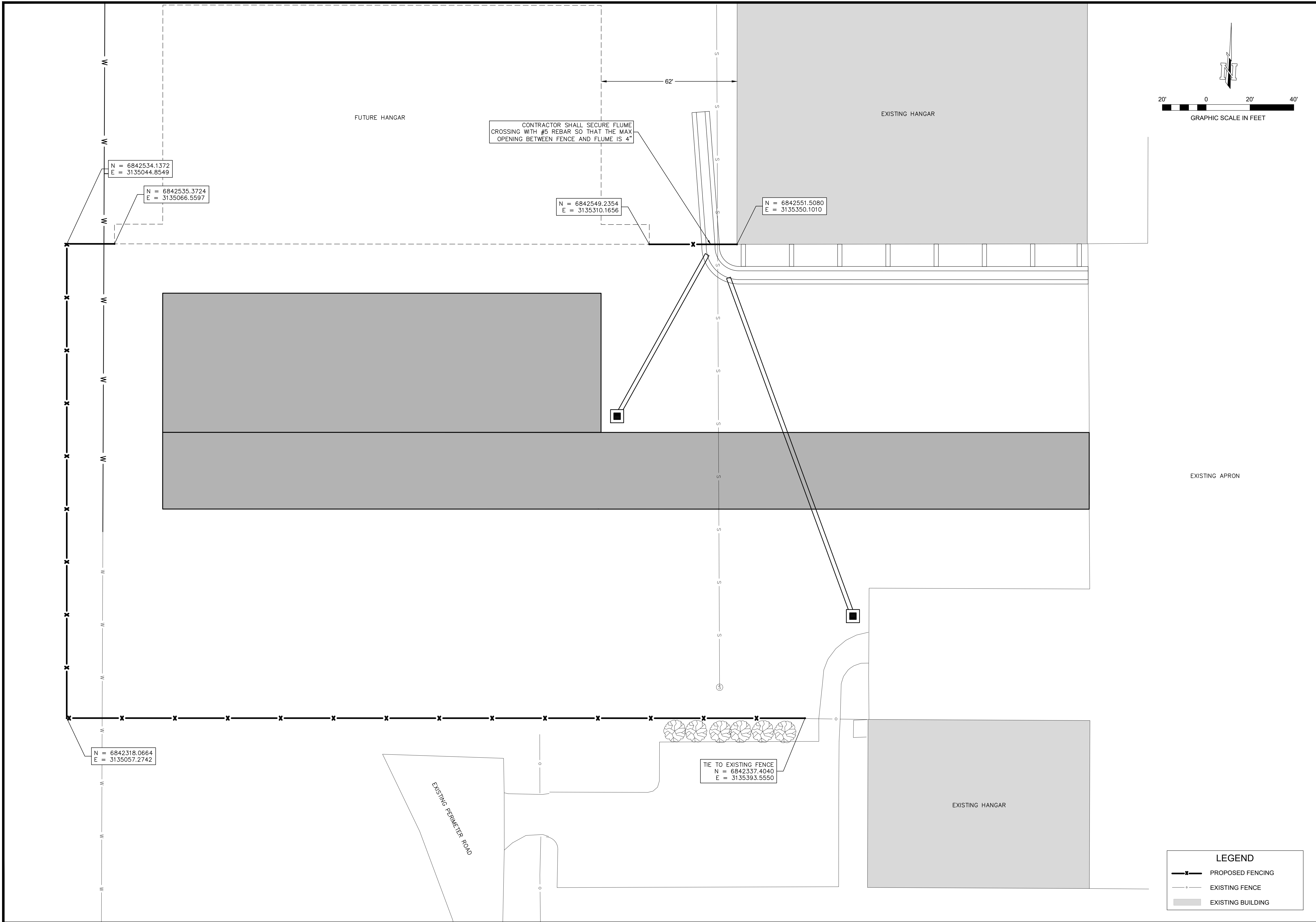
1 TAXIWAY CENTERLINE MARKING DETAIL
N.T.S.



- NOTES:**
- GREEN CENTERLINE TAXIWAY RETROREFLECTIVE MARKERS SHALL BE PLACED:
 - AT EACH PC AND PT OF A CURVE.
 - EVENLY THROUGH A CURVE AT NOT GREATER THAN 25' INTERVALS.
 - EVENLY THROUGH STRAIGHT SECTIONS AT NOT GREATER THAN 50' INTERVALS.
 - RETROREFLECTIVE MARKERS ARE NOT REQUIRED ON APRON/RUNWAY PAVEMENT

2 TYPICAL TAXIWAY CENTERLINE RETROREFLECTIVE MARKERS
N.T.S.

- PAVEMENT MARKING GENERAL NOTES:**
- ALL TAXIWAY CENTERLINE STRIPING, SHALL BE YELLOW RETROREFLECTIVE PAINT AS PER SPECIFICATIONS.
 - ALL TAXIWAY MARKINGS SHALL BE OUTLINED WITH NON-REFLECTIVE BLACK MARKINGS (6" TYP.).
 - HOLD ALL DIMENSIONS FOR STRIPING FROM THE CENTERLINE OF TAXIWAY (AS APPLICABLE).



EXISTING APRON

LEGEND

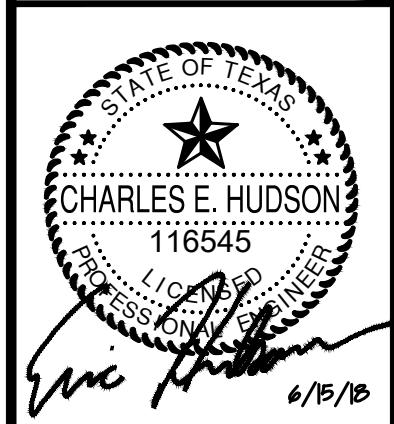
- PROPOSED FENCING
- EXISTING FENCE
- EXISTING BUILDING

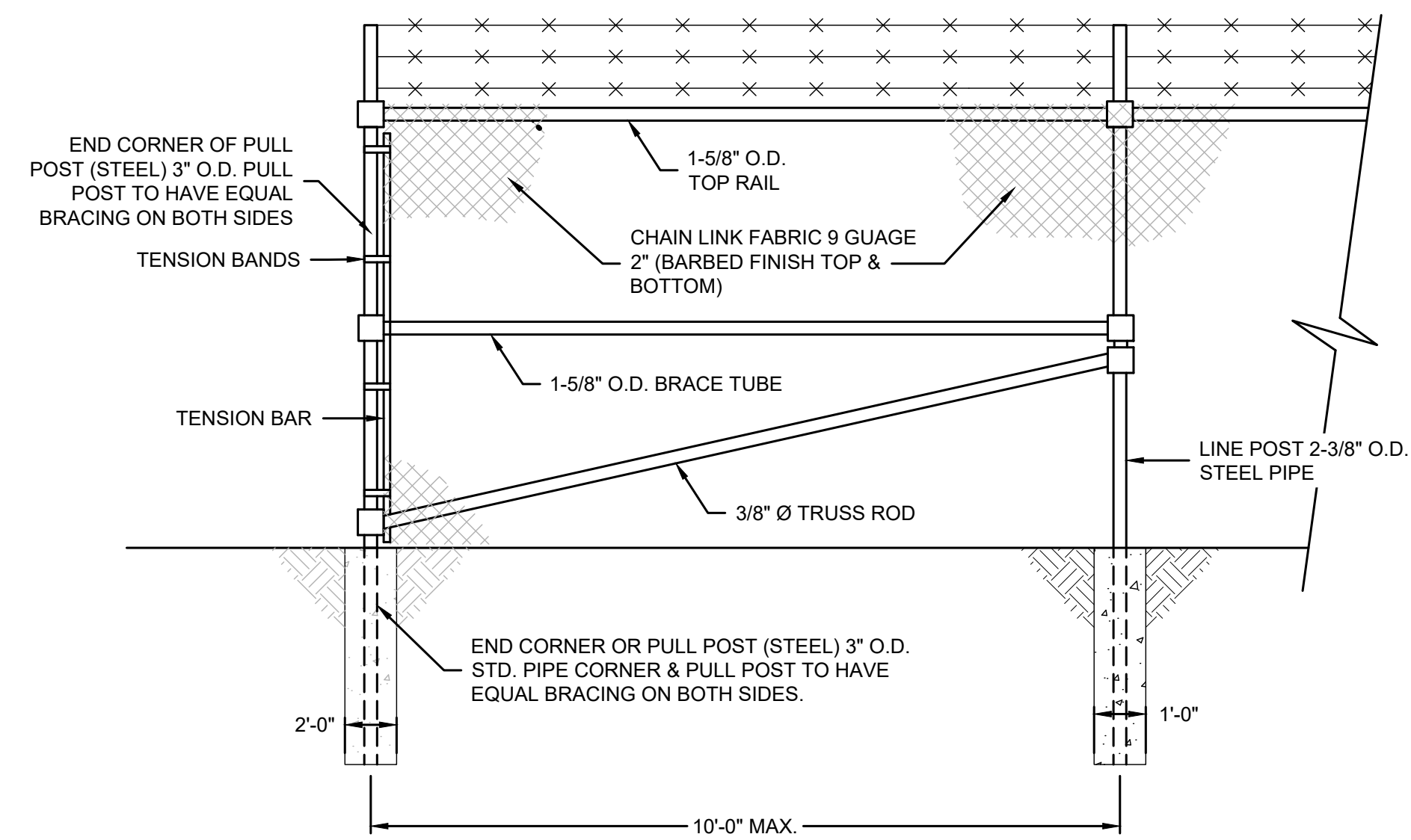
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FENCING PLAN

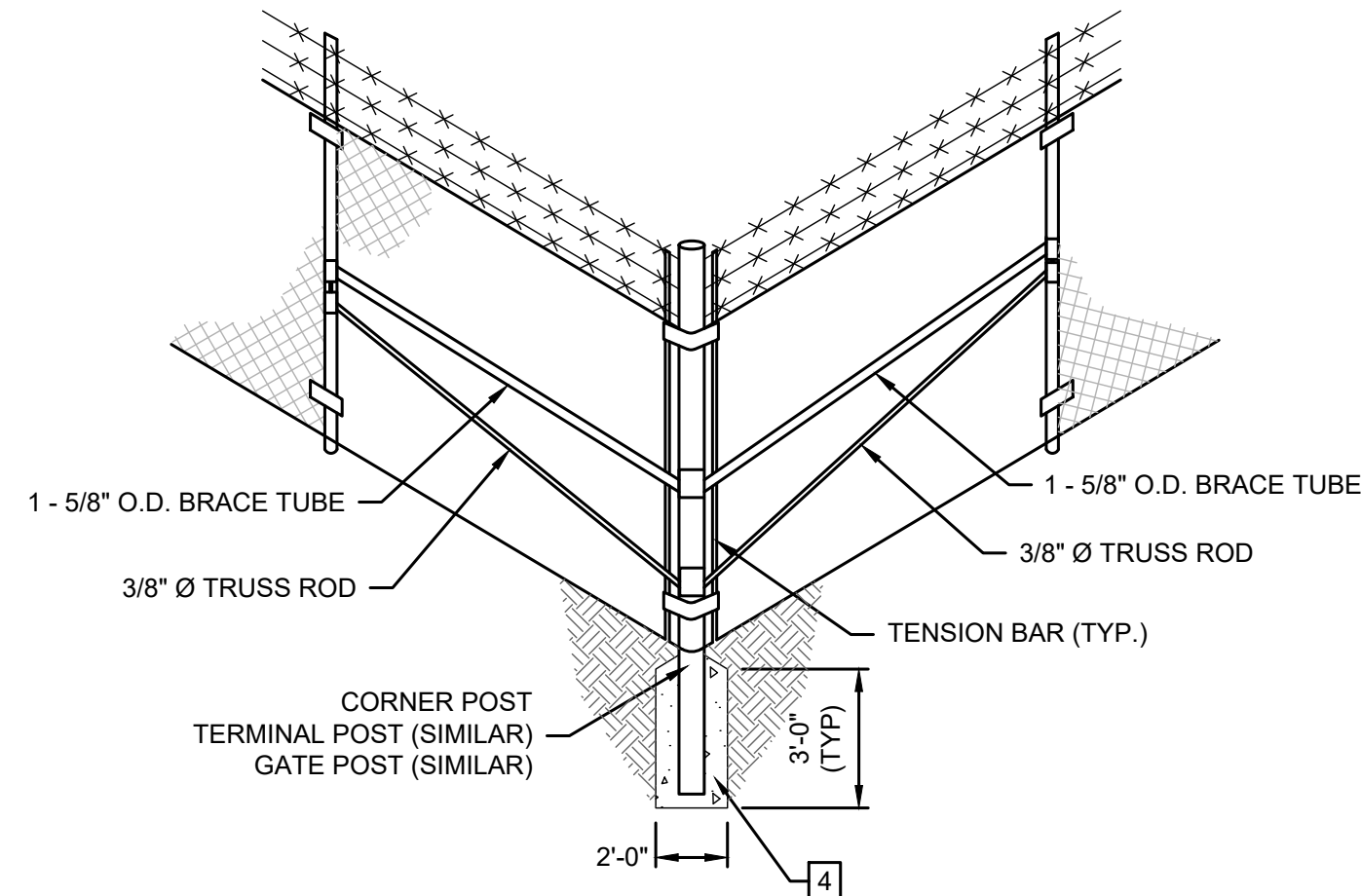
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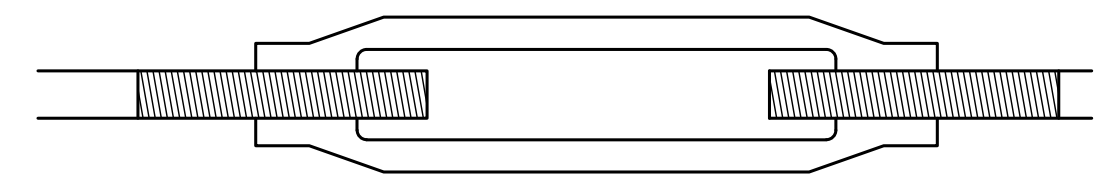
1 CHAIN LINK FENCE DETAIL
N.T.S.



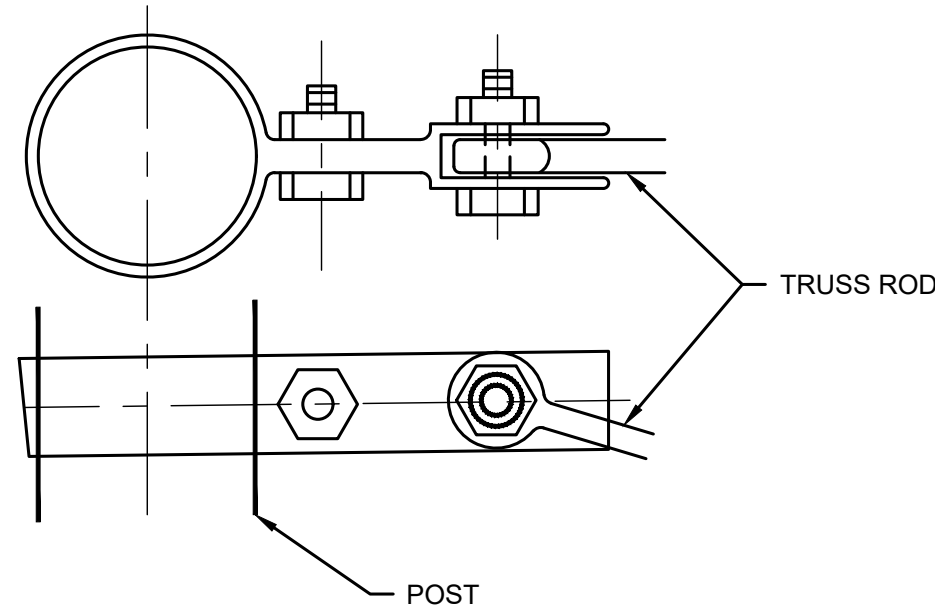
NOTES:

1. PULL POSTS HAVE SIMILAR INSTALLATION.
2. END POSTS HAVE SIMILAR INSTALLATION EXCEPT FOR USE OF TRUSS RODS AND TURNBUCKLES.
3. SPACE PULL POST @ 300' MAXIMUM.
4. CONCRETE SHALL BE P-610

2 CORNER, TERMINAL, AND GATE POST DETAIL
N.T.S.



3 TRUSS ROD AND TURNBUCKLE
N.T.S.



4 TRUSS CONNECTION TO POST
N.T.S.

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FENCING DETAILS I

EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
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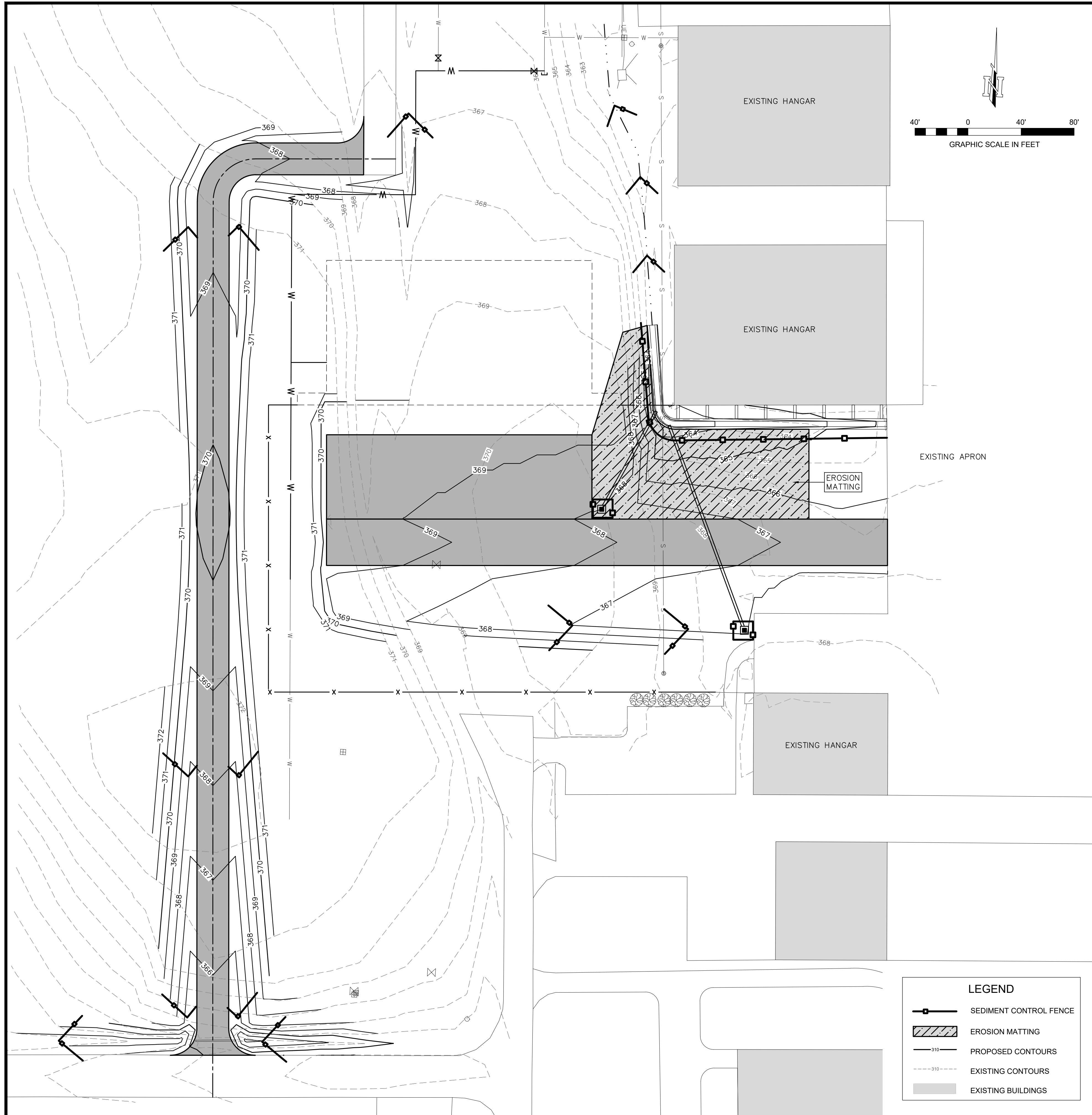
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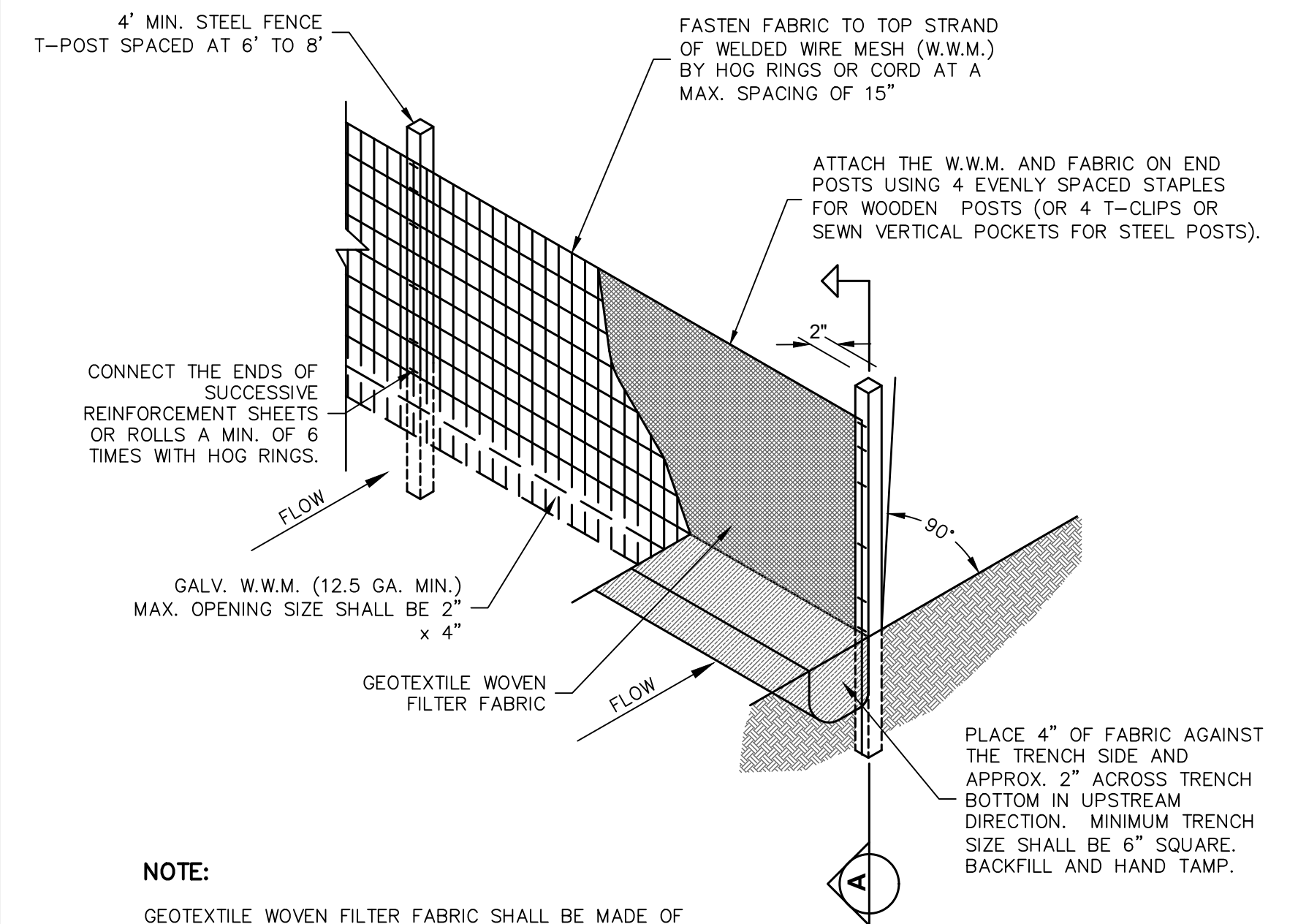
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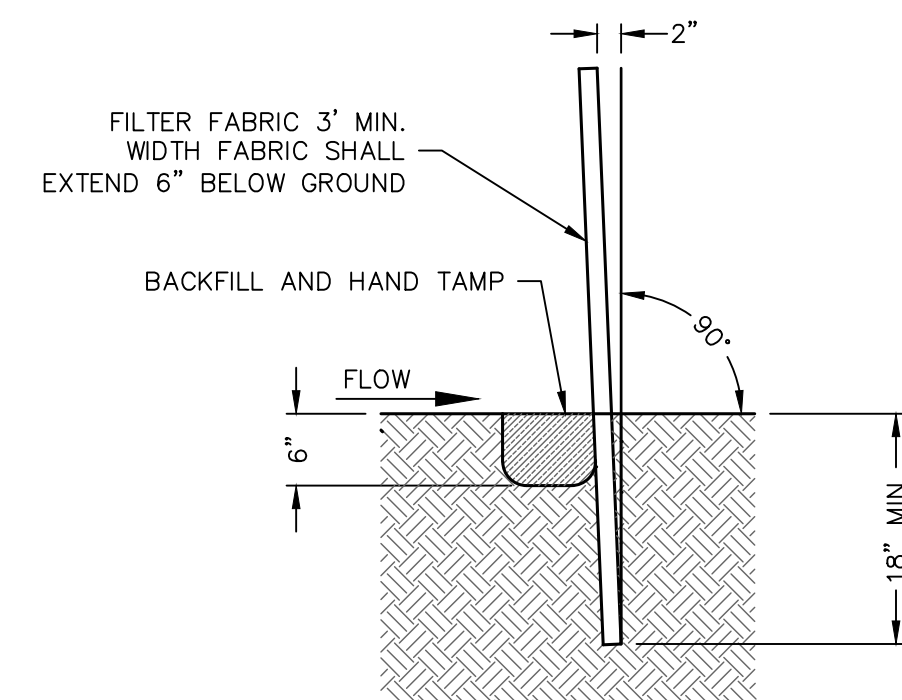


LEGEND

	SEDIMENT CONTROL FENCE
	EROSION MATTING
	PROPOSED CONTOURS
	EXISTING CONTOURS
	EXISTING BUILDINGS



NOTE:
 GEOTEXTILE WOVEN FILTER FABRIC SHALL BE MADE OF EITHER POLYPROPYLENE, POLYETHYLENE, ETHYLENE, OR POLYAMIDE MATERIAL WITH AN EQUIVALENT OPENING SIZE BETWEEN 20 AND 50. IT SHALL HAVE A GRAB STRENGTH OF 100 PSI IN ANY DIRECTION AND A MULLEN BURST STRENGTH EXCEEDING 200 PSI (ASTM D-3786).



SECTION "A"
N.T.S.

NOTES:

- FURNISH AND INSTALL FILTER FABRIC FENCE AT THE REQUIRED LOCATIONS TO KEEP EROSION AND SEDIMENT OFF OF THE STREETS OR PRIVATE PROPERTY, OR AS INSTRUCTED BY THE ENGINEER OR THE ENGINEERING REPRESENTATIVE.
- ALL FILTER FABRIC FENCE PLACEMENT SHALL BE AGREED UPON BY THE ENGINEER OR THE ENGINEERING REPRESENTATIVE.
- CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF FILTER FABRIC FENCE THROUGHOUT CONSTRUCTION ACTIVITIES. ADDITIONAL PAYMENT WILL NOT BE MADE FOR REINSTALLATION OF DAMAGED OR DOWNED FENCE.

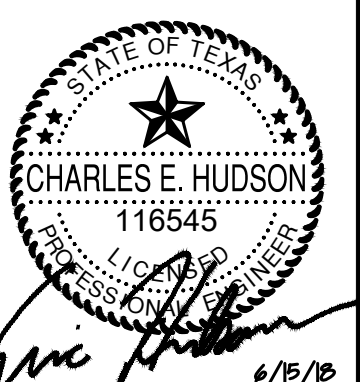
TEMPORARY SEDIMENT CONTROL FENCE
N.T.S.

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EROSION CONTROL PLAN AND DETAILS

**EAST TEXAS REGIONAL AIRPORT
SOUTHWEST GA AREA
TAXILANE - PHASE I
LONGVIEW, TEXAS**

DRAWN BY:	JKP
DESIGNED BY:	CEH
LATEST REVISION:	6/15/2018
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PROJECT TITLE: SOUTHWEST GA AREA TAXILANE - PHASE I - LONGVIEW, TEXAS
SHEET NAME: